



**NOTICE OF A REGULAR
CITY COUNCIL WORK MEETING
July 7, 2026, at 4:00 PM**

PUBLIC NOTICE is hereby given that the Vineyard City Council will hold a regularly scheduled City Council Work meeting on Tuesday, July 7, 2026, at 4:00 PM, in the City Council Chambers at City Hall, 125 South Main Street, Vineyard, UT. This meeting can also be viewed on our [live stream page](#).

AGENDA

1. CALL TO ORDER

2. INVOCATION AND PLEDGE OF ALLEGIANCE (BY INVITATION)

3. PUBLIC COMMENTS

“Public Comments” is defined as time set aside for citizens to express their views. *Public comments can be submitted ahead of time to robinr@vineyardutah.gov.*

4. CONSENT ITEMS

4.1. Approval of the June 23, 2026, City Council Meeting Minutes

5. WORK SESSION

5.1. Discuss 400 South Road Design and Construction (Council Member Holdaway)

Review conceptual roadway alternatives and the approved design for the remaining segment of 400 South, understand the engineering, operational, fiscal, and policy implications associated with each alternative, and receive City Council direction.

5.2. Discuss Motorized Bikes, Scooters, and Other Means of Transportation (Deputy Chief Holden Rockwell)

5.3. Discuss Resolution 2026-31: Insignificant Land Resolution (Council Member Ezra Nair)

6. COMMUNICATION ITEMS

7. ADJOURNMENT

The next regularly scheduled meeting is on July 14, 2026.

This meeting may be held in a way that will allow a council member to participate electronically.

The public is invited to participate in all City Council meetings. In compliance with the Americans with Disabilities Act, individuals needing special accommodations during this meeting should notify the City Recorder at least 24 hours prior to the meeting by calling (385) 338-5183.

I, the undersigned duly appointed City Recorder for Vineyard, Utah, hereby certify that the foregoing notice and agenda was posted at Vineyard City Hall, on the Vineyard City and Utah Public Notice websites, and delivered electronically to staff and to each member of the Governing Body.

July 1, 2026

AGENDA NOTICING COMPLETED ON:

/s/Robin Bond

CERTIFIED (NOTICED) BY:

ROBIN BOND, CITY RECORDER



**NOTICE OF A REGULAR
CITY COUNCIL MEETING
June 23, 2026, at 6:00 PM**

Present:
Mayor Zack Stratton
Council Member Holdaway
Council Member Lauret
Council Member Nair
Council Member Wood

Absent:
Council Member McCumber

Staff Present: Administrative Director David Kyle Herring, Building Inspector/MS4 Coordinator Kinsli McDermott, Building/Planning Director Cris Johnson, Chief Deputy Holden Rockwell with the Utah County Sheriff's Office, City Attorney Jesse Riddle City Manager Brian Voeks, City Planner Anthony Fletcher, City Planner Paul Douglass, City Recorder Robin Bond, Community Enrichment Director Brian Vawdrey, Deputy City Recorder Tony Lara, Finance Director Evan Smith, Library Coordinator Valerie Popadich, Media Specialist Braile Tyler, Project Manager/MS4 Justine Marshall, and Public Works Director/City Engineer Naseem Ghandour

Others Attending: David Peare-Cascade, Jane Pearce-Cascade, Karen Cornelius-Villas, Kim Cornelius-Villas, Christine Jeffs – Vineyard Library Board, Mary Anne Geddes-Vineyard Library Board, Julie Gray-Vineyard Library Board, Katie Tall-Vineyard Library Board, Terry Ewing-Villas, Keith Vincent-Villas, Rosslyn Valdivieso-Latino Coalition, and others who did not sign in or whose writing was illegible.

AGENDA

Presiding Mayor Zack Stratton

1. CALL TO ORDER

Mayor Stratton called the meeting to order at 6:03 pm.

**2. INVOCATION, INSPIRATIONAL THOUGHT, AND PLEDGE OF ALLEGIANCE
(BY INVITATION)**

David Hyte offered the invocation and led the Pledge of Allegiance.

**3. PUBLIC HEARING FOR MUNICIPAL CODE UPDATES FOR STORMWATER
MANAGEMENT**

A public hearing was held regarding proposed updates to Chapter 7.08 of the Municipal Code related to stormwater management. No public comments were made.

4. PUBLIC HEARING FOR THE FINAL FISCAL YEAR 2026-2027 BUDGET

A public hearing was held regarding the Fiscal Year 2026–2027 Budget. No public comments were made.

43 5.  **RECOGNITIONS/AWARDS**

44 **5.1. Recognize the Library Staff and Library Board for Achieving Certification**
45 **(Brian Vawdrey)**

46 Brian Vawdrey recognized the Library Coordinator and Library Board for achieving State
47 Library Certification following three years of effort. Certification will provide access to
48 additional resources and grant opportunities.

49
50 **6. PUBLIC COMMENTS**

51 **“Public Comments”** is defined as time set aside for citizens to express their views. *Public*
52 *comments can be submitted ahead of time to robinr@vineyardutah.gov.*

53 No comments.

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55 **7. CONSENT ITEMS**

56 **7.1. Approval of the June 9, 2026, City Council Meeting Minutes**

57 **7.2. Ordinance 2026-06, Floodplain Ordinance Update**

58 **MOTION:** Council Member Holdaway motioned to approve the consent items as
59 presented. Council Member Wood **seconded** the motion. **Yes:** Council Members
60 Holdaway, Lauret, Nair, and Wood. **No:** None. **Motion passed 4-0.**

61
62 **8. BUSINESS ITEMS**

63 **8.1. Approve Ordinance 2026-09, Stormwater Management Code Update (Naseem**
64 **Ghandour)**

65 Conduct a public hearing and consider adoption of an ordinance amending Chapter
66 7.08 of the Vineyard City Municipal Code relating to stormwater management. The
67 proposed amendments update the City's stormwater regulations to align with current
68 UPDES MS4 permit requirements, establish regulatory authority, and provide
69 standards for stormwater utility administration, construction oversight, post-
70 construction management, and enforcement.

71 Naseem Ghandour presented a PowerPoint presentation on the needed code updates.
72 He thanked the Assistant Engineer Patrick James, Justine Marshall, and Kinsli
73 McDermott for their efforts on this project. Mr. Ghandour presented proposed
74 amendments to Chapter 7.08 to comply with State and UPDES (Utah Pollutant
75 Discharge Elimination System) MS4 (Municipal Separate Storm Sewer system) permit
76 requirements. Updates establish regulatory authority and standards for stormwater
77 utility administration, construction oversight, post-construction management, and
78 enforcement. Staff conducted public outreach through social media and the city
79 newsletter.

80 Council discussion included reporting procedures for storm drain concerns and the
81 importance of protecting waterways, wildlife, and agricultural animals from stormwater
82 contamination.

83 Mr. Ghandour said that the public could contact the public works department or the fire
84 department to report storm drain issues. “Only rain in the drain.”, is the goal.

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MOTION: Council Member Holdaway motioned to approve Ordinance 2026-09, Stormwater Management Code Update as presented. **Seconded by** Council Member Lauret. **Yes:** Council Members Holdaway, Lauret, Nair, and Wood. **No:** None. **Motion Passed 4-0.**

8.2.  Accept the Final FY 2026-2027 Budget (Evan Smith)

Evan Smith presented budget updates and noted the proposed budget was lower than the previous fiscal year. The Council discussed property tax impacts, reserve funding, capital projects, and departmental reductions.

Council Member Nair proposed three alternative budgets to lower property taxes for residents. He suggested continuing the budget item to a future meeting.

Council Member Lauret proposed adding funding for improvements to the 400 South bike boulevard project. Council members generally supported the addition.

MOTION: Council Member Holdaway motioned to approve the Fiscal Year 2026-2027 Budget with an amendment for the road project suggested by Council Member Lauret. **Seconded by** Council Member Wood. **Yes:** Council Member Holdaway, Lauret, and Wood. **No:** Nair. **MOTION PASSED 3-1.**

8.3. Approve Fee Waiver for a Latino Festival Special Event on 9.19.26. (Brian Vawdray)

Brian Vawdray presented a request to waive approximately \$3,035 in City fees for the Latino Festival scheduled for September 19, 2026, at Grove Park. Event organizers explained the festival is free to the public, celebrates cultural diversity, and is funded through vendor fees to cover event costs. Approximately 3,000 attendees are anticipated.

Council members expressed support for the event and encouraged City promotion and public safety coordination.

MOTION: Council Member Holdaway motioned to approve the Fee Waiver for the Latino Festival Special Event 8.3. and would like the City to help promote this event on September 19, 2026. **Seconded by** Council Member Wood **Yes:** Council Members Holdaway, Lauret, Nair, and Wood. **No:** None. **MOTION PASSED 4-1.**

9. CLOSED SESSION

The Mayor and City Council, pursuant to Utah Code 52-4-205, may vote to go into a closed session for the purpose of personnel, real property, or litigation.

No closed session was held.

10. ADJOURNMENT

124 Mayor Stratton adjourned at 7:11 pm.

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130 **MINUTES APPROVED ON:** July 7, 2026

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133 **CERTIFIED CORRECT BY:** _____

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ROBIN BOND, CITY RECORDER

DRAFT



VINEYARD CITY COUNCIL STAFF REPORT

Meeting Date: July 7, 2026

Agenda Item: Discuss 400 South Road Design and Construction (Council Member Holdaway)

Department: Public Works

Presenter:

Background/Discussion:

The remaining segment of 400 South between Holdaway Road and the Holdaway Fields development is the final section needed to complete the east-west transportation corridor identified in the Vineyard Transportation Master Plan. Most of the corridor has already been designed and partially constructed in accordance with the City's adopted roadway standards, the Transportation Master Plan, and the Holdaway Fields Development Agreement (**Attachment 3**).

Completion of the remaining segment requires the acquisition of approximately 400 square feet (sf) of right-of-way from an adjacent property owner to construct the planned roadway cross-section (**Attachment 2**). Over the past year, City staff have worked to acquire the necessary right-of-way; however, those efforts have not resulted in an agreement.

Subsequently, Councilmember Holdaway requested a series of meetings with City staff to present and discuss a conceptual roadway configuration for consideration. During this process, City staff met separately with Councilmember Holdaway, Councilmember Lauret, Councilmember Wood (with no meeting including more than two councilmembers), and a resident along 400 South to discuss roadway geometry, bicycle and pedestrian accommodations, corridor operations, parking concerns, and long-term transportation needs.

At the direction of Councilmember Holdaway, City staff documented the conceptual roadway configuration presented during these meetings (**Exhibit 1 of Attachment 1**), planning-level sketch rather than an engineered roadway design. City staff subsequently developed additional conceptual alternatives (**Exhibits 2, 4, and 5 of Attachment 1**) that incorporated City engineering standards, Fire Department access requirements, the Holdaway Fields Development Agreement, the Transportation Master Plan, and other applicable criteria. These alternatives, along with the current approved designs (**Exhibit 3 of Attachment 1**), are intended to provide the City Council with a complete engineering evaluation of the available options and their associated impacts outlined in **Table 1**.

Separate from the roadway cross-section discussions, Councilmembers and nearby residents also expressed concerns regarding vehicles parking within the existing bicycle lanes along 400 South along Lakeside Park near the Vineyard/Orem city boundary. City staff has been coordinating with Orem City regarding additional red curb installation and parking enforcement. These operational improvements are being pursued independently and are not dependent upon the roadway cross-section ultimately selected.

The concepts presented in this report are planning-level exhibits intended to facilitate policy discussion. Final engineering design would proceed only after the City Council provides direction, with the exception of the Current Approved concept, as construction designs are complete.

Alternative Comparison, Table 1

CONSIDERATIONS	Councilman Holdaway's Concept (Without city design standards)	Councilman Holdaway's Concept (With city design standards)	Current Approved Design	Cycle Track Alternative 1	Cycle Track Alternative 2
Meets City Engineering Standards	✗	✓	✓	✓	✓
Meets Fire Dept Pavement Width	✗	✓	✓	✓	✓
Consistent with Holdaway Fields Development Agreement	✗	✗	✓	✓	✓
Maintains Pedestrian Continuity	✗	✓	✓	✓	✓
Compatible with Orem City's 400 South Segment	✗	✓	✓	✗	✗
Allows North Crosswalk at S Holdaway Rd	✗	✓	✓	✓	✓
Maintains Intersection Alignment	✗	✓	✓	✓	✓
Additional ROW Required	Yes	Yes	Yes	Yes	Yes
Construction This Season	✗	✗	✓	✗	✗
Additional Design Cost	approx. \$30,000	approx. \$30,000	None	approx. \$30,000	approx. \$30,000
Design Delay	approx. 2 months	approx. 2 months	None	approx. 2 months	approx. 2 months
Estimated Reconstruction Cost	approx. \$250,000	approx. \$125,000	\$0	approx. \$125,000	approx. \$125,000
Utility Relocation Cost	approx. \$100,000	approx. \$5,000	\$0	\$0	\$0
Compatible with the 2025 Transportation Master Plan	✗	✗	✓	✓	✓
Estimated Additional City Cost	approx. \$380,000	approx. \$160,000	\$0	approx. \$155,000	approx. \$155,000
Estimated Completion Date	2027-2028	2027-2028	2026	2027-2028	2027-2028
Overall Compatibility	Low	Medium	High	Medium	Medium

Schedule Impact

The current approved design is the option capable of meeting the 2026 construction schedule and the asphalt paving deadline of October 15, 2026, provided that property acquisition proceeds.

All other alternatives require approximately two months of redesign before construction can begin, delaying completion until the 2027–2028 construction season, depending on funding approvals.

Summary of Each Concept/Alternative:

Alternative 1: Councilmember Holdaway Concept (Without City Design Standards)

Description

This alternative represents the conceptual roadway configuration documented by City staff at the direction of Councilmember Holdaway (Exhibit 1). The concept reflects the roadway configuration presented during stakeholder meetings and does not incorporate current City engineering or roadway design standards. The Transportation Master Plan identifies the road segment as a collector road, which would require revising the roadway master plan for alternative routes or planning for future roadway reconstruction to meet the city's future transportation demands.

Summary

Requires acquisition of approximately 40 square feet of right-of-way.

Requires redesign and reconstruction of completed improvements.

Requires future alternative roadway connection(s) or future reconstruction 400 South project
Estimated additional City cost: approximately \$380,000 (not including future reconstruction costs).

Construction Season: approximately 2027-2028

Alternative 2 – Councilmember Holdaway Concept (With City Design Standards)

Description

This alternative modifies Councilmember Holdaway's concept to incorporate applicable City engineering and roadway standards while maintaining, where practical, the design intent discussed during stakeholder meetings. The Transportation Master Plan identifies the road segment as a collector road, which would require revising the roadway master plan for alternative routes or planning for future roadway reconstruction to meet the city's future transportation demands.

Summary

Incorporates City engineering standards into the requested concept.

Requires acquisition of approximately 400 square feet of right-of-way.

Requires redesign and partial reconstruction of completed improvements.

Estimated additional City cost: approximately \$160,000 (not including future reconstruction costs).

Construction Season: approximately 2027-2028

Alternative 3 – Current Approved Design

Description

This alternative is the roadway currently designed, approved, and partially constructed by the City. It is consistent with the Transportation Master Plan, the Holdaway Fields Development Agreement, and the City's adopted engineering standards. The Transportation Master Plan identifies the road segment as a collector road, which would require no new construction but restriping during a scheduled pavement resurfacing to meet the city's future transportation

demands.

Summary

Preserves the current approved roadway design.
Requires acquisition of approximately 400 square feet of right-of-way.
Preserves existing design and construction investments.
Estimated additional City cost: \$0
Construction Season: approximately 2026

Alternative 4 – Cycle Track Alternative 1

Description

This alternative replaces the traditional bicycle lane configuration with a separated cycle track while maintaining the City's roadway standards and planned roadway corridor. The Transportation Master Plan identifies the road segment as a collector road, which would require no new construction but restriping during a scheduled pavement resurfacing to meet the city's future transportation demands.

Summary

Maintains City engineering standards using a cycle track configuration.
Requires acquisition of approximately 400 square feet of right-of-way.
Requires redesign and partial reconstruction of completed improvements.
Estimated additional City cost: approximately \$155,000 (not including future reconstruction costs).
Construction Season: approximately 2027-2028

Alternative 5 – Cycle Track Alternative 2

Description

This alternative evaluates a modified cycle track configuration intended to reduce reconstruction while maintaining City engineering standards.

Summary

Modified cycle track alternative.
Requires acquisition of approximately 400 square feet of right-of-way.
Requires redesign and partial reconstruction of completed improvements.
Estimated additional City cost: approximately \$155,000 (not including future reconstruction costs).
Construction Season: approximately 2027-2028

Conclusion

This report presents the conceptual roadway alternatives evaluated by City staff, summarizes the engineering and operational considerations associated with each alternative, and identifies the anticipated cost and schedule impacts. City Council's policy direction will allow staff to proceed with final engineering, right-of-way acquisition, and project implementation consistent with the Council's preferred direction.

Fiscal Impact:

The City has invested approximately \$165,000 in engineering design and construction for the currently approved roadway configuration.

1. Alternative 1: approximately \$380,000 in additional funds (exclusive of inflation)
2. Alternative 2: approximately \$160,000 in additional funds (exclusive of inflation)
3. Alternative 3: \$0 in additional funds

4. Alternatives 4 and 5: approximately \$155,000 in additional duns (exclusive of inflation)

Recommendation:

None

Sample Motion:

None

Attachments:

1. Concepts - 400 South Holdaway Road Segment
2. Preliminary ROW Exhibit
3. 400 South Vineyard Design Plans - For Construction

Councilman Holdaway's Concept

Without city design standards applied

400 South (West Side)

400 South (East Side)

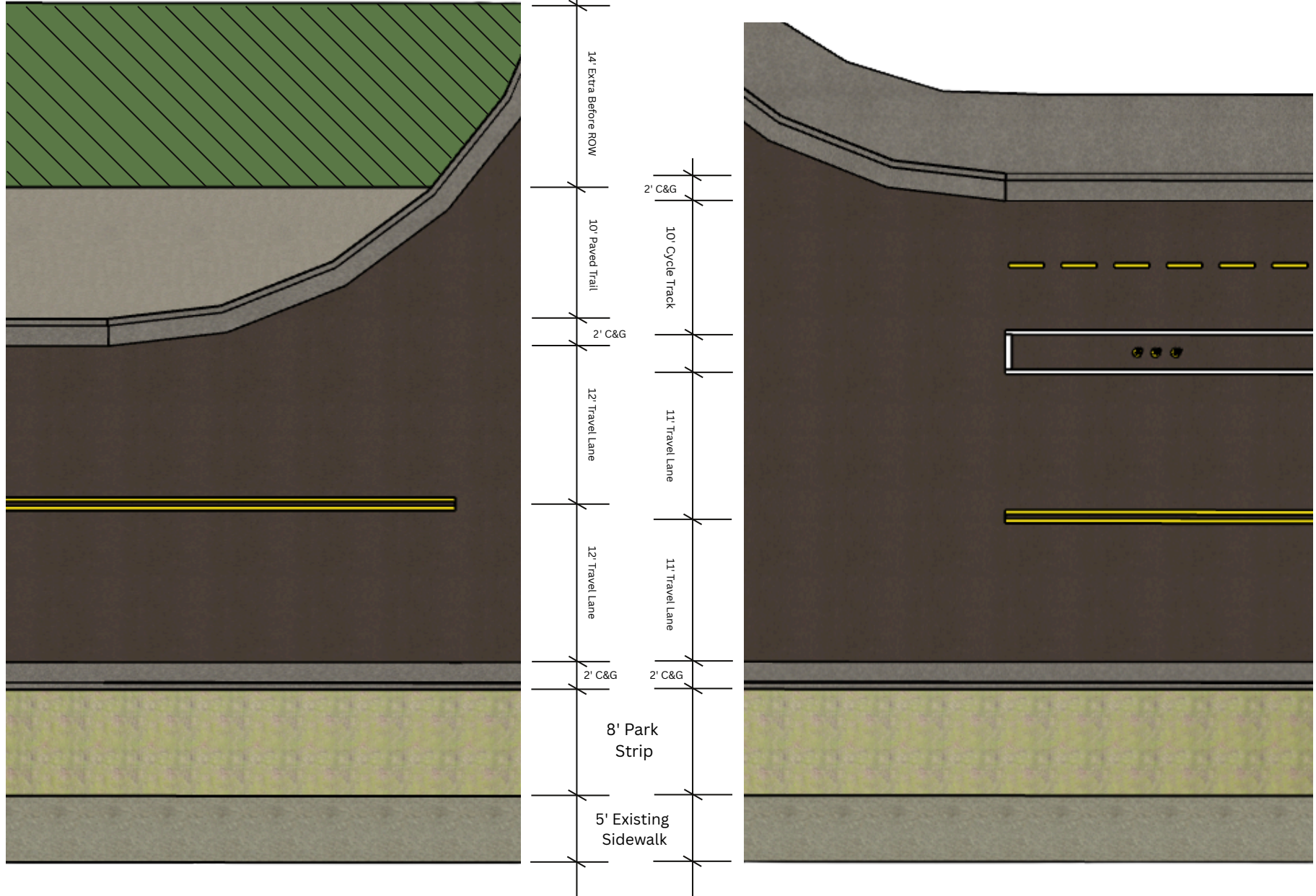


EXHIBIT 1

Councilman Holdaway's Concept

With city design standards applied

400 South (West Side)

400 South (East Side)

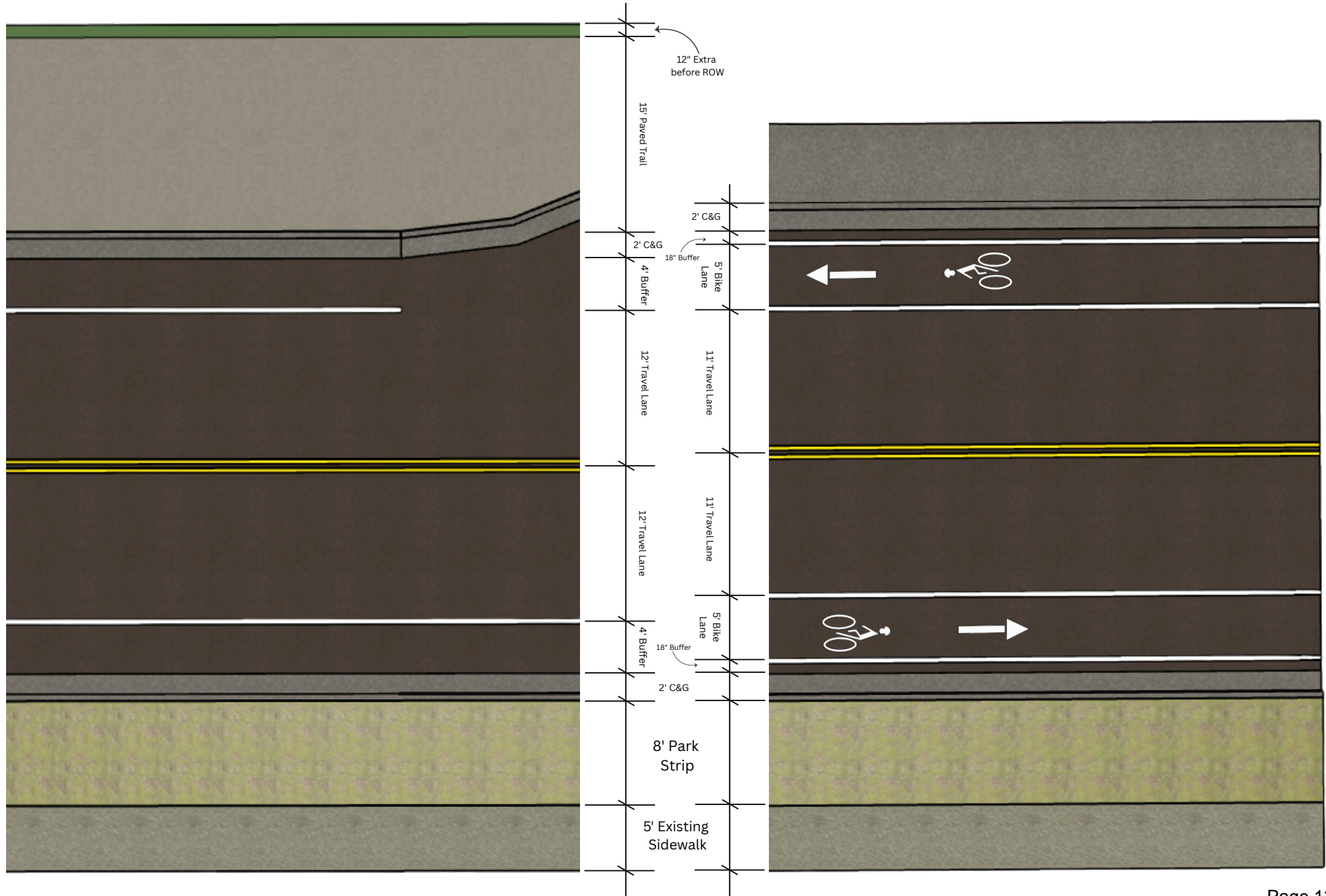
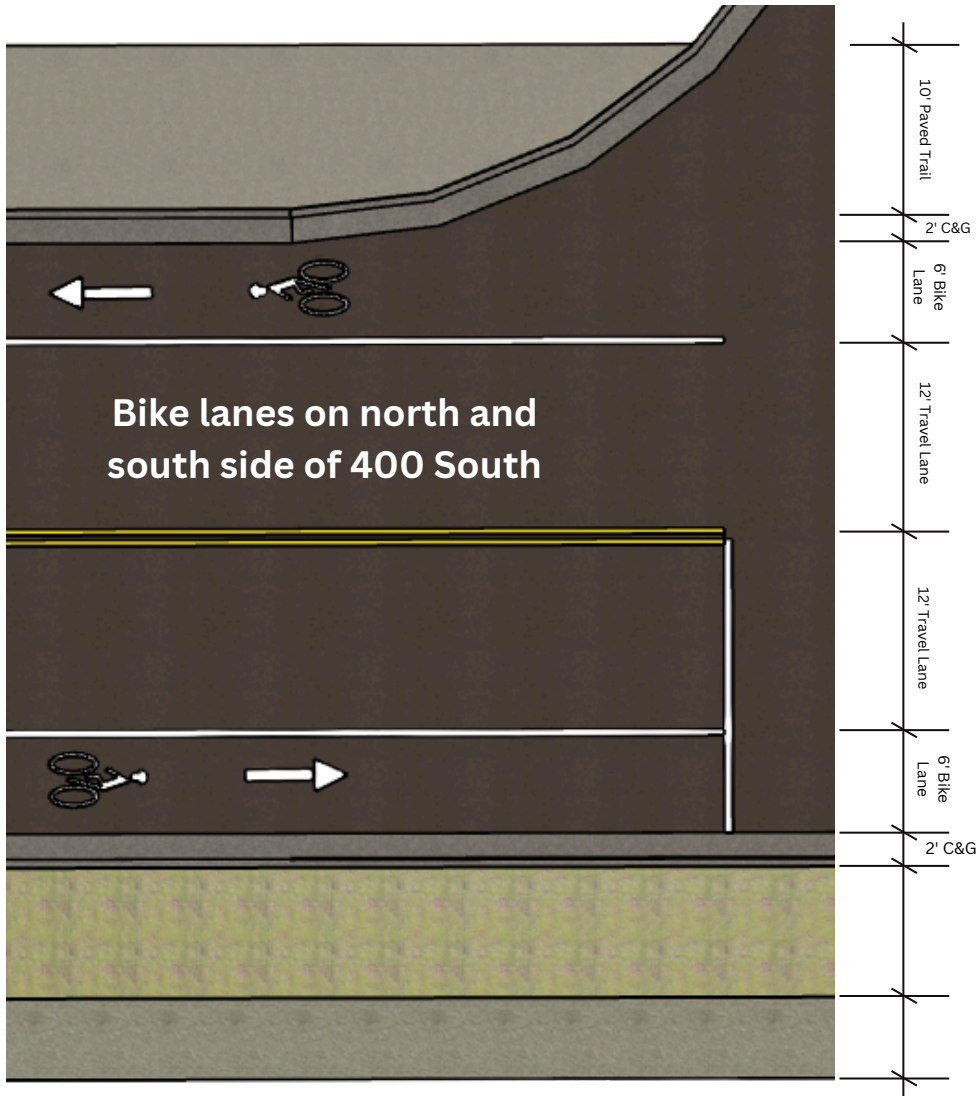


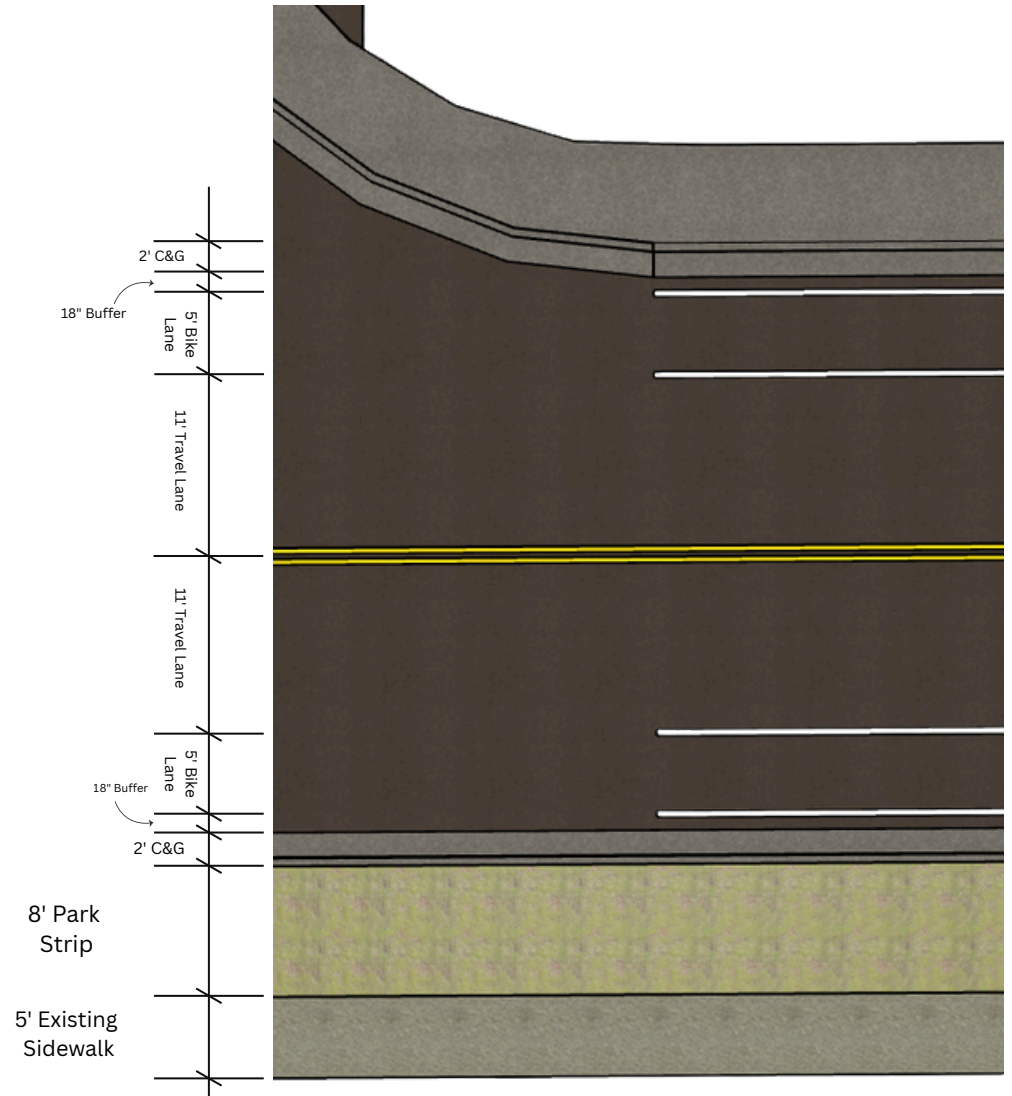
EXHIBIT 2

Current Approved Design

400 South (West Side)



400 South (East Side)



Bike lanes on north and south side of 400 South

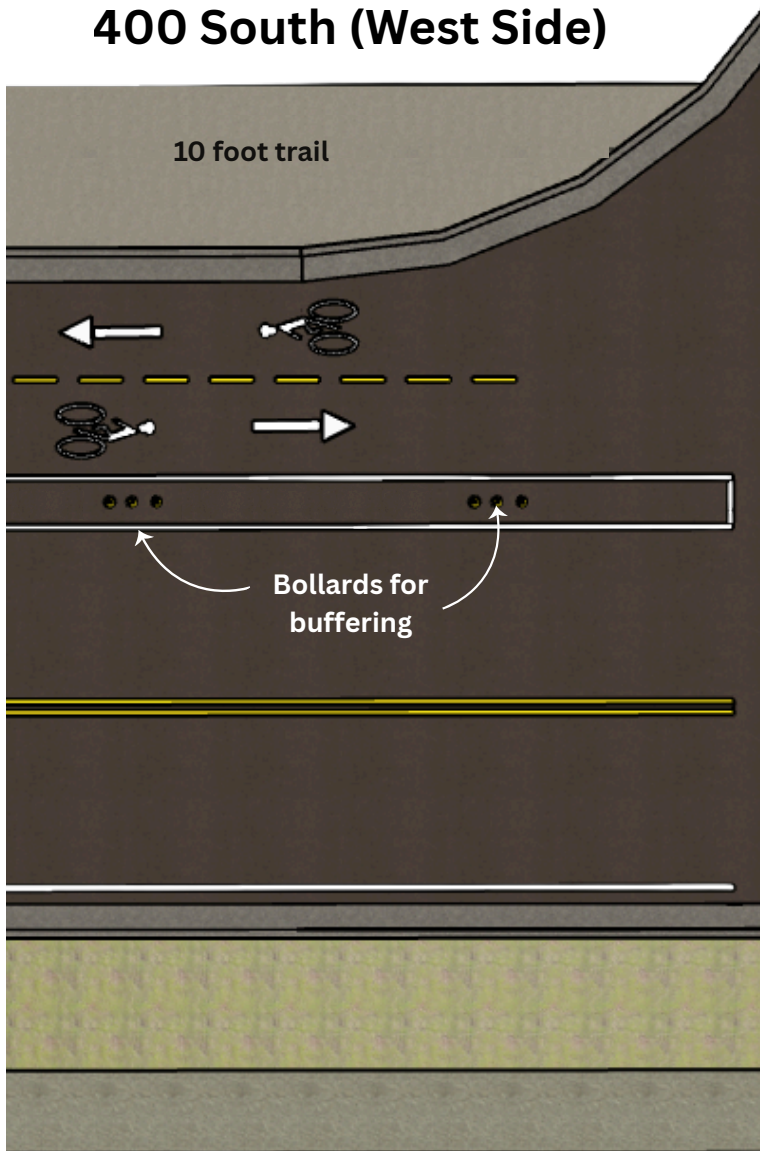
EXHIBIT 3

Cycle Track Alternative 1

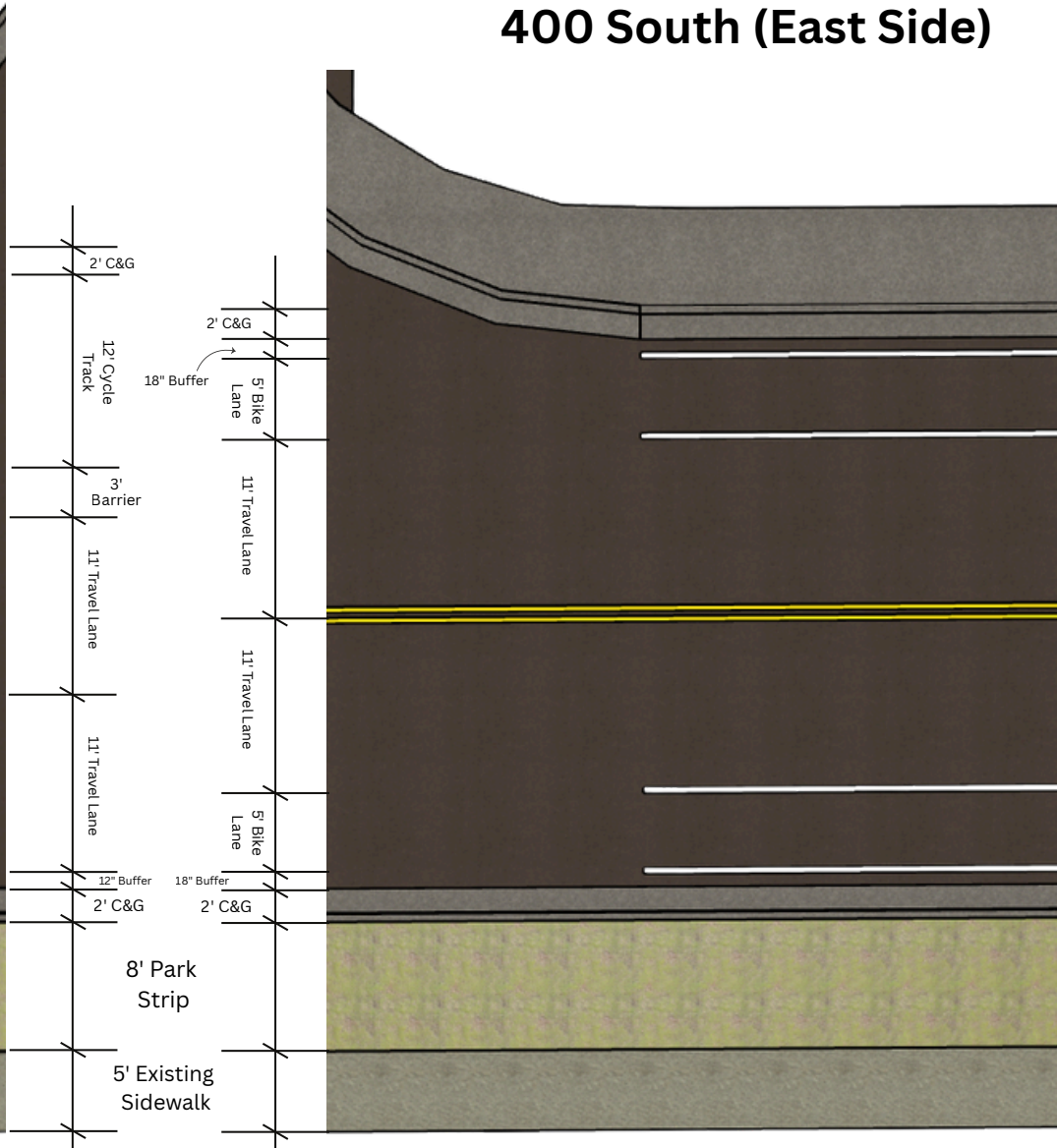
With current east side striping (Orem recommended)

EXHIBIT 4

400 South (West Side)



400 South (East Side)



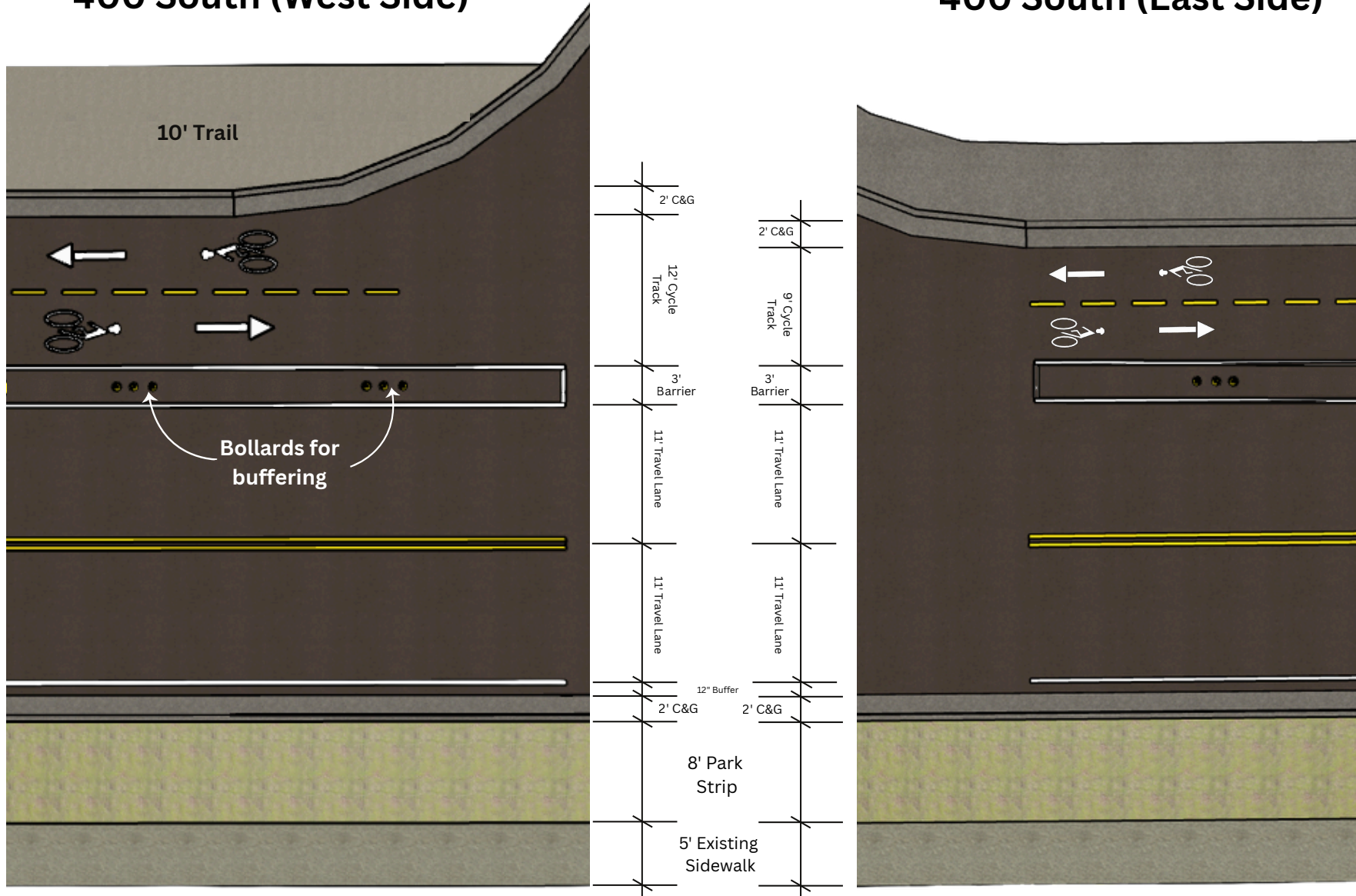
Cycletrack Alternative 2

Without Orem's east side striping recommendation

400 South (West Side)

400 South (East Side)

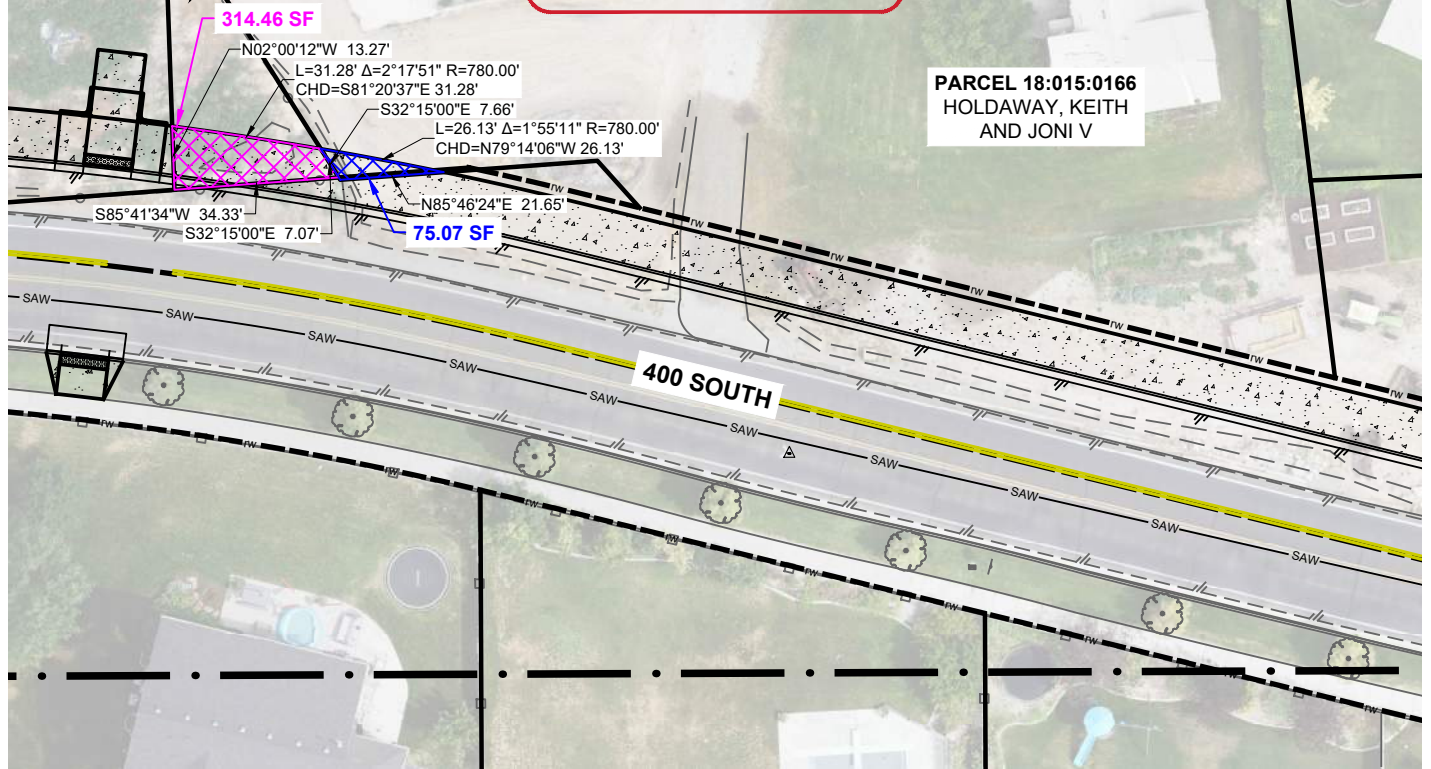
EXHIBIT 5



**PRELIMINARY
NOT FOR FILING
PROPERTY
ACQUISITION**

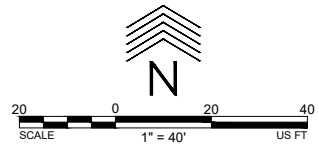
**PARCEL 18:021:0010
HOLDAWAY, KEITH JONI V**

**PARCEL 18:015:0166
HOLDAWAY, KEITH
AND JONI V**



PARCEL 1 – LEGAL DESCRIPTION:
 BEGINNING AT A POINT ON THE EXISTING PARCEL BOUNDARY OF PARCEL 18:021:0010, SAID POINT BEING S85°41'34"W 34.33 FEET AND S32°15'00"E 7.07 FEET FROM THE NORTHWEST CORNER OF SAID PARCEL; THENCE N02°00'12"W 13.27 FEET; THENCE ALONG A CURVE TO THE RIGHT WITH A RADIUS OF 780.00 FEET, AN ARC LENGTH OF 31.28 FEET, SAID CURVE HAVING A CENTRAL ANGLE OF 2°17'51" AND A CHORD BEARING S81°20'37"E 31.28 FEET; THENCE S32°15'00"E 7.66 FEET; THENCE S85°41'34"W 34.33 FEET TO THE POINT OF BEGINNING.
 CONTAINING 314.46 SQUARE FEET, MORE OR LESS.

PARCEL 2 – LEGAL DESCRIPTION:
 BEGINNING AT A POINT ON THE SOUTHEASTERLY LINE OF PARCEL 18:021:0010, SAID POINT BEING N85°46'24"E 21.65 FEET FROM THE SOUTHWEST CORNER OF SAID PARCEL; THENCE ALONG A CURVE TO THE RIGHT WITH A RADIUS OF 780.00 FEET, AN ARC LENGTH OF 26.13 FEET, SAID CURVE HAVING A CENTRAL ANGLE OF 1°55'11" AND A CHORD BEARING N79°14'06"W 26.13 FEET; THENCE S32°15'00"E 7.07 FEET; THENCE N85°46'24"E 21.65 FEET TO THE POINT OF BEGINNING.
 CONTAINING 75.07 SQUARE FEET, MORE OR LESS.



avenue CONSULTANTS

Avenue Consultants, Inc
588 W 400 S, Suite 250
Lindon, UT 84042
www.avenueconsultants.com

400 SOUTH ROAD RECONSTRUCTION

Drawn By | KAF

RIGHT-OF-WAY ACQUISITION

SHEET NO.
1 OF 2

SHEET TITLE
EXHIBIT

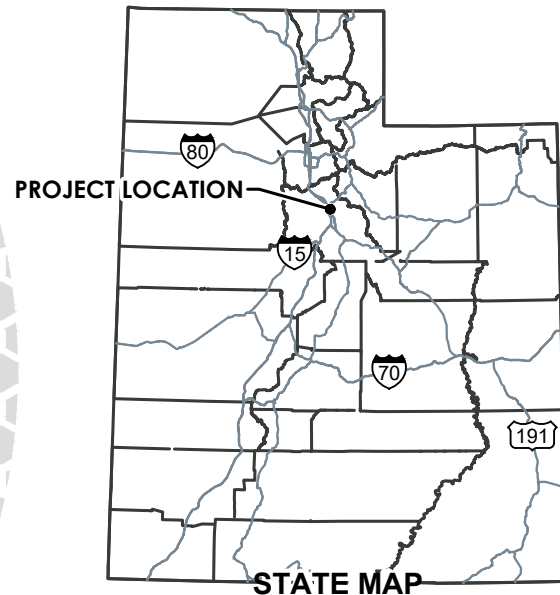
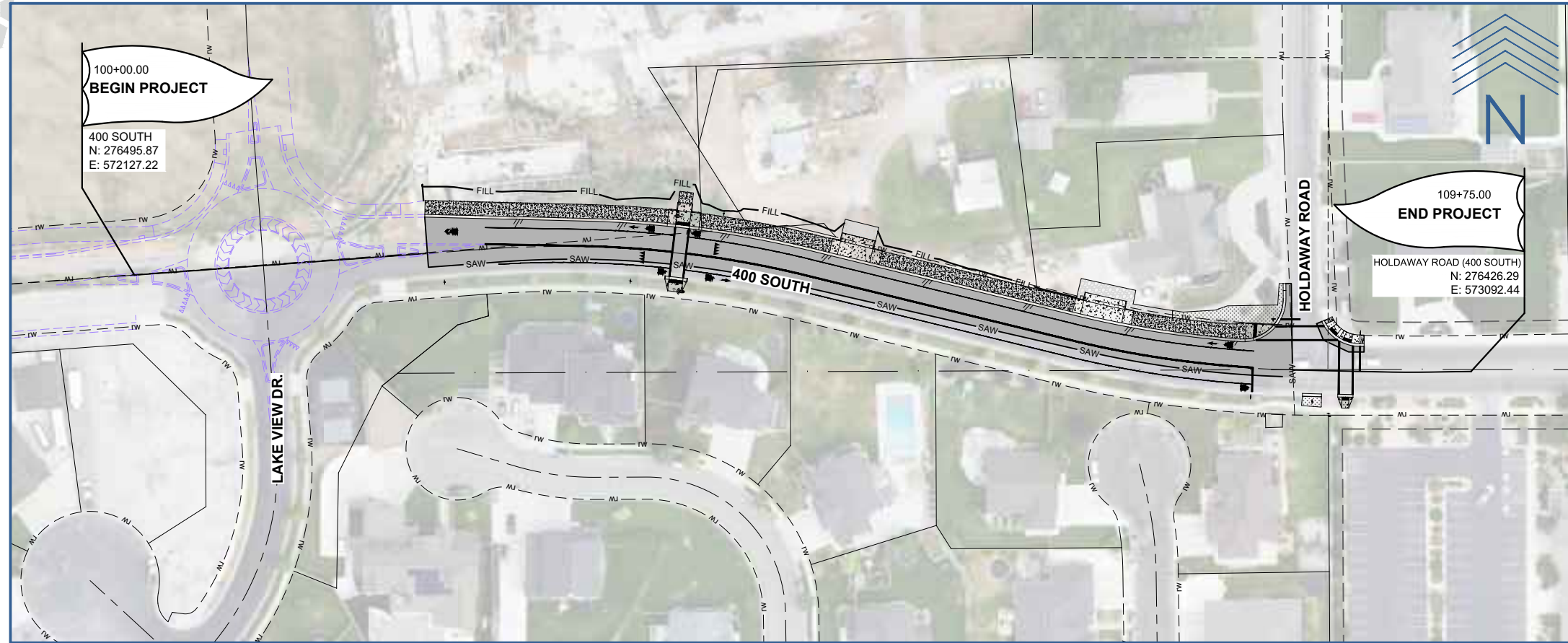
VINEYARD, UTAH



400 S RECONSTRUCTION (HOLDAWAY RD TO LAKEVIEW DR)

13 DECEMBER 2025

VICINITY MAP



SHEET LIST TABLE

SHEET NUMBER	SHEET TITLE	SHEET DESCRIPTION
1	G001	TITLE SHEET
2	G002	GENERAL NOTES AND ABBREVIATIONS
3	CS100	SURVEY CONTROL
4	RMV-01	REMOVAL
5	RMV-02	REMOVAL
6	RD-01	ROADWAY
7	RD-02	ROADWAY
8	GR-01	GRADING & DRAINAGE
9	GR-02	GRADING & DRAINAGE
10	RDT-01	ROADWAY DETAILS
11	RDT-02	ROADWAY DETAILS
12	RDT-03	ROADWAY DETAILS
13	RDT-04	ROADWAY DETAILS
14	RDT-05	ROADWAY DETAILS
15	RDT-06	ROADWAY DETAILS
16	RDT-07	ROADWAY DETAILS
17	RDT-08	ROADWAY DETAILS
18	RDT-09	ROADWAY DETAILS

PROJECT CONTACTS

OWNER	CIVIL ENGINEER / SURVEYOR
VINEYARD CITY 125 S. MAIN STREET VINEYARD, UT 84059	AVENUE CONSULTANTS 588 W 400 S, SUITE 250 LINDON, UT 84042
CONTACT: NASEEM GHANDOUR PHONE: (385) 338-5281 EMAIL: NASEEM@VINEYARDUTAH.GOV	CONTACT: KIMBERLY FOSTER, P.E., LSIT PHONE: (801) 860-7536 EMAIL: KFOSTER@AVENUECONSULTANTS.COM

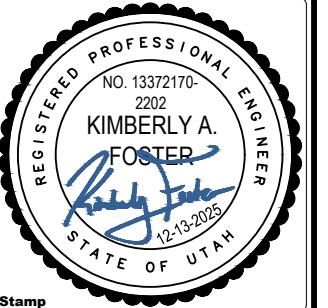


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REVISIONS:

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TITLE SHEET

VINEYARD CITY

**400 S RECONSTRUCTION
(HOLDAWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

SHEET NO. 1 OF 18

SHEET TITLE **G001**

Avenue Project No. 25-343

p:_2025\05-343 - 400 south roadway improvements\00_cadd\05_sheets\25-343_g001 - title sheet.dwg

12/13/2025

THE CONTRACTOR SHALL CAREFULLY READ ALL OF THE NOTES AND SPECIFICATIONS. THE CONTRACTOR SHALL BE SATISFIED AS TO THEIR TRUE MEANING AND INTENT AND SHALL BE RESPONSIBLE FOR COMPLYING WITH EACH.

CONTRACTOR TO COMMUNICATE WITH PROPERTY OWNERS WITH THE OWNER, OR OWNER'S REPRESENTATIVE.

GENERAL NOTES:

- 1) ALL IMPROVEMENTS SHALL BE CONSTRUCTED IN STRICT ACCORDANCE WITH ALL JURISDICTIONAL AUTHORITIES.
2) CONTRACTOR SHALL COMPLY WITH THE STANDARDS INDICATED ABOVE AND WITHIN THE CONSTRUCTION DOCUMENTS.
3) IT IS INTENDED THAT THESE PLANS AND SPECIFICATIONS REQUIRE ALL LABOR AND MATERIALS NECESSARY AND PROPER FOR THE WORK CONTEMPLATED AND THAT THE WORK BE COMPLETED IN ACCORDANCE WITH THEIR TRUE INTENT AND PURPOSE.
4) WHERE THE PLANS OR SPECIFICATIONS DESCRIBE PORTIONS OF THE WORK IN GENERAL TERMS BUT NOT IN COMPLETE DETAIL, IT IS UNDERSTOOD THAT ONLY THE BEST GENERAL PRACTICE IS TO PREVAIL AND THAT ONLY MATERIALS AND WORKMANSHIP OF THE FIRST QUALITY ARE TO BE USED.
5) THE CONTRACTOR SHALL BE SKILLED AND REGULARLY ENGAGED IN THE GENERAL CLASS AND TYPE OF WORK CALLED FOR IN THE PROJECT PLANS AND SPECIFICATIONS.
6) THE CONTRACTOR SHALL BE COMPETENT, KNOWLEDGEABLE, AND HAVE SPECIAL SKILLS ON THE NATURE, EXTENT, AND INHERENT CONDITIONS OF THE WORK TO BE PERFORMED.
7) THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND LICENSES REQUIRED FOR THE CONSTRUCTION AND COMPLETION OF THE PROJECT.
8) CONTRACTOR SHALL OBTAIN AN ENCROACHMENT PERMIT WHERE APPLICABLE FOR ANY WORK DONE WITHIN RIGHTS-OF-WAY OR EASEMENTS FROM VINEYARD CITY.
9) THE CONTRACTOR SHALL, AT THE TIME OF BIDDING, AND, THROUGHOUT THE PERIOD OF THE CONTRACT, BE LICENSED IN THE STATE OF UTAH AND SHALL BE BONDABLE FOR AN AMOUNT EQUAL TO OR GREATER THAN THE AMOUNT BID AND TO DO THE TYPE OF WORK CONTEMPLATED IN THE PLANS AND SPECIFICATIONS.
10) CONTRACTOR SHALL INSPECT THE SITE OF THE WORK PRIOR TO BIDDING TO SATISFY THEMSELVES BY PERSONAL EXAMINATION OR BY SUCH OTHER MEANS AS THEY MAY PREFER, OF THE LOCATION OF THE PROPOSED WORK, AND OF THE ACTUAL CONDITIONS OF, AND AT, THE SITE OF WORK.
11) THE CONTRACTOR SHALL PROVIDE ALL LIGHTS, BARRICADES, SIGNS, FLAGGERS OR OTHER DEVICES NECESSARY FOR PUBLIC SAFETY.
12) THE CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE ALL WATER, POWER, SANITARY FACILITIES AND TELEPHONE SERVICES AS REQUIRED FOR THE CONTRACTORS USE DURING CONSTRUCTION.
13) THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY FIELD CHANGES MADE WITHOUT PRIOR WRITTEN AUTHORIZATION FROM THE OWNER AND/OR ENGINEER.
14) THE CONTRACTOR SHALL EXERCISE DUE CAUTION AND SHALL CAREFULLY PRESERVE BENCH MARKS, CONTROL POINTS, SECTION CORNERS, REFERENCE POINTS AND ALL SURVEY STAKES, AND SHALL BEAR ALL EXPENSES FOR REPLACEMENT AND/OR ERRORS CAUSED BY UNNECESSARY LOSS OR DISTURBANCE.
15) THE CONTRACTOR AGREES THAT:
A) THEY SHALL BE RESPONSIBLE TO CLEAN THE JOB SITE AT THE END OF EACH DAY.
B) THEY SHALL BE RESPONSIBLE TO REMOVE AND DISPOSE OF ALL TRASH, SCRAP AND UNUSED MATERIAL AT THEIR OWN EXPENSE IN A TIMELY MANNER.
C) THEY SHALL BE RESPONSIBLE TO MAINTAIN THE SITE IN A NEAT, SAFE AND ORDERLY MANNER AT ALL TIMES.
D) THEY SHALL BE RESPONSIBLE TO KEEP MATERIALS, EQUIPMENT, AND TRASH OUT OF THE WAY OF OTHER CONTRACTORS SO AS NOT TO DELAY THE JOB. FAILURE TO DO SO WILL RESULT IN A DEDUCTION FOR THE COST OF CLEAN UP FROM THE FINAL PAYMENT.
E) THEY SHALL BE RESPONSIBLE FOR THEIR OWN SAFETY, TRAFFIC CONTROL, PERMITS, RETESTING AND RE-INSPECTIONS AT THEIR OWN EXPENSE.
16) THE CONTRACTOR SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOBSITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY.
17) DUST CONTROL SHALL BE PROVIDED AT ALL TIMES, AT THE CONTRACTOR'S EXPENSE, TO MINIMIZE ANY DUST NUISANCE AND SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF VINEYARD CITY.
18) WHEN CONSTRUCTION STAKING IS REQUIRED THE CONTRACTOR SHALL NOTIFY THE ENGINEER/LAND SURVEYOR ONE (1) WEEK IN ADVANCE OF THE NEED FOR STAKING. ANY STAKING REQUESTED BY THE CONTRACTOR OR THEIR SUBCONTRACTORS THAT IS ABOVE AND BEYOND STANDARD STAKING NEEDS, INCLUDING RE-STAKING WILL BE SUBJECT TO A CONTRACTOR CHANGE ORDER AND THE IMPACT COSTS OF WORK PERFORMED.
19) FOR ALL WORK WITHIN PUBLIC RIGHTS-OF-WAYS OR EASEMENTS, THE CONTRACTOR SHALL PRESERVE THE INTEGRITY AND LOCATION OF ANY AND ALL PUBLIC UTILITIES AND PROVIDE THE NECESSARY CONSTRUCTION TRAFFIC CONTROL. CONTRACTOR SHALL, THROUGH THE ENCROACHMENT PERMIT PROCESS, VERIFY WITH THE NECESSARY REGULATORY AGENCIES, THE NEED FOR ANY TRAFFIC ROUTING PLAN. IF PLAN IS REQUIRED, CONTRACTOR SHALL PROVIDE PLAN AND RECEIVE PROPER APPROVALS PRIOR TO BEGINNING CONSTRUCTION.

- 20) THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADEQUATELY SCHEDULING INSPECTION AND TESTING OF ALL FACILITIES CONSTRUCTED UNDER THIS CONTRACT. ALL TESTING SHALL CONFORM TO THE REGULATORY AGENCY'S STANDARD SPECIFICATIONS. ALL RE-TESTING AND/OR RE-INSPECTION SHALL BE PAID FOR BY THE CONTRACTOR.
21) IF EXISTING FEATURES NEED TO BE DISTURBED AND/OR REMOVED FOR THE PROPER PLACEMENT OF IMPROVEMENTS TO BE CONSTRUCTED BY THESE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING FEATURES FROM DAMAGE. COST OF REPLACING OR REPAIRING EXISTING FEATURES SHALL BE INCLUDED IN THE BID PRICE FOR ITEMS REQUIRING REMOVAL AND/OR REPLACEMENT.
22) THE CONTRACTOR SHALL MAINTAIN A NEATLY MARKED SET OF FULL-SIZE AS-BUILT DRAWINGS SHOWING THE FINAL LOCATION AND LAYOUT OF ALL FACILITIES. AS-BUILT DRAWINGS SHALL REFLECT CHANGE ORDERS, ACCOMMODATIONS, AND ADJUSTMENTS TO ALL IMPROVEMENTS CONSTRUCTED. WHERE NECESSARY, SUPPLEMENTAL DRAWINGS SHALL BE PREPARED AND SUBMITTED BY THE CONTRACTOR. PRIOR TO ACCEPTANCE OF THE PROJECT, THE CONTRACTOR SHALL DELIVER TO THE ENGINEER, ONE SET OF NEATLY MARKED AS-BUILT DRAWINGS SHOWING THE INFORMATION REQUIRED ABOVE. AS-BUILT DRAWINGS SHALL BE REVIEWED AND THE COMPLETE AS-BUILT DRAWING SET SHALL BE CURRENT WITH ALL CHANGES AND DEVIATIONS REDLINED AS A PRECONDITION TO THE FINAL PROGRESS PAYMENT APPROVAL AND/OR FINAL ACCEPTANCE.
23) WORK IN EASEMENT AND/OR RIGHTS-OF-WAY IS SUBJECT TO THE APPROVAL AND ACCEPTANCE OF THE REGULATORY AGENCY RESPONSIBLE FOR OPERATION AND/OR MAINTENANCE OF SAID EASEMENT AND/OR RIGHTS-OF-WAY.
24) NO ALLOWANCE WILL BE MADE FOR DISCREPANCIES OR OMISSIONS THAT CAN BE EASILY OBSERVED. VERIFY ALL EXISTING CONDITIONS BEFORE BIDDING, AND ANSWER ANY QUESTIONS BEFORE CONSTRUCTION.
25) FURNISH, MAINTAIN, AND RESTORE ALL MONUMENTS AND MONUMENT REFERENCE MARKS WITHIN THE PROJECT SITE. CONTACT THE CITY OR COUNTY SURVEYOR FOR MONUMENT LOCATIONS AND CONSTRUCTION DETAILS.
26) PROVIDE A CONSTRUCTION SCHEDULE IN ACCORDANCE WITH CITY, COUNTY, AND STATE REGULATIONS FOR WORKING IN THE RIGHT-OF-WAY.
27) FURNISH ALL MATERIALS TO COMPLETE THE PROJECT.
28) TRAFFIC CONTROL IS TO CONFORM TO THE CURRENT MUTCD AND UDOT STANDARDS.
29) CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ADJACENT SURFACE IMPROVEMENTS.
30) ALL EXISTING ASPHALT WILL BE SAW CUT IN NEAT STRAIGHT LINES BY THE CONTRACTOR PRIOR TO EXCAVATION.
31) STRIPING WILL BE PER THE PLANS AND/OR AS DIRECTED BY THE OWNER'S REPRESENTATIVE. STRIPING TO INCLUDE HANDICAP INSIGNIAS, SIGNS, CROSS-HATCHING, DIRECTION ARROWS, ETC. AS SHOWN OR AS DIRECTED.

GENERAL CLEARING AND GRADING NOTES:

- 1) CLEARING, GRUBBING AND DISPOSAL OF VEGETATIVE MATERIAL NEEDS TO BE IN ACCORDANCE WITH STATE AND COUNTY REGULATIONS, WHICH APPLY TO SOLID WASTE.
2) CONTRACTOR SHALL PERFORM EARTHWORK IN ACCORDANCE WITH VINEYARD CITY STANDARD SPECIFICATIONS AND CONSTRUCTION STANDARDS, EROSION, SEDIMENT, RE-VEGETATION REQUIREMENTS, AND THE SWPP PLAN AS REQUIRED BY THE STATE OF UTAH, DEPARTMENT OF ENVIRONMENTAL QUALITY AND DIVISION OF AIR QUALITY.
3) THE CONTRACTOR WILL PERFORM EARTHWORK IN ACCORDANCE WITH TECHNICAL SPECIFICATIONS OUTLINED IN THE CONSTRUCTION STANDARDS AND PER CURRENT CITY STANDARD.
4) SEDIMENTATION BMP'S SHOWN ON THE EROSION CONTROL AND SEDIMENT CONTROL PLANS (STORM WATER POLLUTION PREVENTION PLAN) TO BE INSTALLED WITHIN THE SAME WORKING DAY THE LAND DISTURBANCE OCCURS.
5) DUST CONTROL BMP'S ARE TO BE ON SITE AND IMPLEMENTED AS SOON AS LAND DISTURBANCE OCCURS.
6) THE EXISTING TOPOGRAPHY SHOWN ON THESE PLANS IS BASED ON DRAWINGS AND A TOPOGRAPHIC SURVEY PERFORMED BY AVENUE CONSULTANTS. IF THE EXISTING GRADE IS DIFFERENT FROM WHAT IS SHOWN ON THE GRADING PLAN, STOP WORK AND CONTACT AVENUE CONSULTANTS. WORK IS TO REMAIN STOPPED UNTIL THE ENGINEER PROVIDES A WRITTEN NOTICE TO RESUME WORK.
7) THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE STREETS, STORM DRAINS, CHANNELS, DITCHES, AND SWALES FREE FROM DEBRIS, SOIL, MUD, OR OTHER MATERIAL THAT WOULD CAUSE A PUBLIC SAFETY CONCERN OR VIOLATE ANY CITY, STATE, OR FEDERAL LAWS.
8) BMP'S ARE TO BE IN PLACE AND MAINTAINED UNTIL WRITTEN NOTIFICATION IS RECEIVED FROM VINEYARD CITY.
9) IF DISTURBANCE OCCURS OUTSIDE THE LIMITS OF DISTURBANCE, WORK WILL STOP AND REMAIN STOPPED UNTIL A WRITTEN RESPONSE IS RECEIVED FROM THE ENGINEER.
10) THE CONTRACTOR SHALL REMOVE ALL VEGETATION AND DELETERIOUS MATERIALS FROM THE SITE UNLESS NOTED OTHERWISE.
11) NATURAL VEGETATION AND SOIL COVER SHALL NOT BE DISTURBED PRIOR TO ACTUAL CONSTRUCTION OF A REQUIRED FACILITY OR IMPROVEMENT. MASS CLEARING OF THE SITE IN ANTICIPATION OF CONSTRUCTION SHALL BE AVOIDED. CONSTRUCTION TRAFFIC SHALL BE LIMITED TO ONE APPROACH TO SITE. THE APPROACH SHALL BE DESIGNATED BY THE OWNER.

GENERAL UTILITY NOTES

- 1) THE LOCATIONS OF UNDERGROUND FACILITIES SHOWN ON THESE PLANS ARE BASED ON FIELD SURVEYS AND LOCAL UTILITY COMPANY RECORDS. IT SHALL BE THE CONTRACTORS FULL RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY COMPANIES TO LOCATE THEIR FACILITIES PRIOR TO PROCEEDING WITH CONSTRUCTION. NO ADDITIONAL COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR DAMAGE AND REPAIR TO THESE FACILITIES CAUSED BY CONTRACTORS WORK FORCE.
2) START AT THE LOW END OF ALL GRAVITY FED LINES AND WORK UPHILL. FAILURE TO COMPLY WITH THIS NOTE SHALL RELEASE THE CIVIL ENGINEER OF ALL LIABILITY.
3) CONTRACTOR SHALL LAYOUT AND POTHOLE FOR ALL POTENTIAL CONFLICTS WITH UTILITY LINES ON OR OFF SITE AS REQUIRED PRIOR TO ANY CONSTRUCTION.
4) STORM DRAIN - SEE VINEYARD CITY STANDARDS & SPECIFICATIONS FOR ALL DETAILS & SPECIFICATIONS GOVERNING THE CONSTRUCTION & INSPECTION OF THE STORM DRAIN & APPURTENANCES WITHIN THE PUBLIC RIGHT-OF-WAY AS SHOWN ON THIS PLAN. SEE THE DETAILS PROVIDED ON THIS SET OF DRAWINGS FOR ALL OTHER STORM DRAIN CONSTRUCTION. APWA STANDARDS WILL BE USED IN THE ABSENCE OF ANY STANDARDS AND DETAILS.
5) ALL DIMENSIONS AND GRADES OF EXISTING STORM DRAIN PIPES, BOX CULVERTS, STRUCTURES, ETC. SHOWN ON THE PLANS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. NOTIFY THE ENGINEER IF ANY DISCREPANCIES EXIST, PRIOR TO PROCEEDING WITH CONSTRUCTION FOR NECESSARY PLAN OR GRADE CHANGES. NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO THE DIMENSIONS OR GRADES SHOWN INCORRECTLY ON THESE PLANS, IF SUCH NOTIFICATION HAS NOT BEEN GIVEN.
6) THESE PLANS SHOW THE LOCATION OF POWER, NATURAL GAS, AND COMMUNICATIONS UTILITIES, BUT ARE NOT DESIGN DRAWINGS FOR THE RELOCATION OR REMOVAL OF EXISTING DRY UTILITIES, NOR FOR ANY NEW DRY UTILITY STUBS. CONTRACTOR IS TO SUBMIT SITE PLAN TO DRY UTILITIES FOR DESIGN OF SERVICE CONNECTIONS TO BUILDING. ACTUAL CONSTRUCTION OF SAID SERVICES TO BE DONE BY RESPECTIVE UTILITY PROVIDERS.
7) VERIFY DEPTHS OF UTILITIES IN THE FIELD BY POT HOLING A MINIMUM OF 300 FEET AHEAD OF PIPELINE CONSTRUCTION TO AVOID CONFLICTS WITH DESIGNED PIPELINE GRADE AND ALIGNMENT. IF A CONFLICT ARISES RESULTING FROM THE CONTRACTOR'S NEGLIGENCE TO POTHOLE UTILITIES THE CONTRACTOR WILL BE REQUIRED TO RESOLVE THE CONFLICT WITHOUT ADDITIONAL COST OR CLAIM TO THE OWNER OR ENGINEER.
8) ALL DIMENSIONS, GRADES, AND UTILITY DESIGNS SHOWN ON THE PLANS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. NOTIFY THE ENGINEER IF ANY DISCREPANCIES EXIST, PRIOR TO PROCEEDING WITH CONSTRUCTION FOR NECESSARY PLAN OR GRADE CHANGES. NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO THE DIMENSIONS OR GRADES SHOWN INCORRECTLY ON THESE PLANS, IF SUCH NOTIFICATION HAS NOT BEEN GIVEN.
9) NO CHANGE IN DESIGN LOCATION OR GRADE WILL BE MADE BY THE CONTRACTOR WITHOUT THE WRITTEN

ABBREVIATIONS

- APWA AMERICAN PUBLIC WORKS ASSOCIATION
AWWA AMERICAN WATER WORKS ASSOCIATION
B&C BAR & CAP
BLA BOUNDARY LINE AGREEMENT
BLDG BUILDING
BM BENCHMARK
BOW BACK OF WALK
BP BURIED POWER
BV BUTTERFLY VALVE
BVC BEGIN VERTICAL CURVE
C&G CURB AND GUTTER
CB CATCH BASIN
CH CHORD
CI CAST IRON
CIP CAST IN PLACE
CL CENTERLINE
CMP CORRUGATED METAL PIPE
CO CLEANOUT
COMM COMMUNICATIONS
CONC CONCRETE
CW CULINARY WATER
DI DUCTILE IRON
EOA EDGE OF ASPHALT
EG EXISTING GRADE
EL ELEVATION, ELBO
ELEV ELEVATION
EOP EDGE OF PAVEMENT
EVC END VERTICAL CURVE
EX EXISTING
FFE FINISH FLOOR ELEVATION
FG FINISH GRADE
FH FIRE HYDRANT
FL FLOWLINE, FLANGE
FT FEET
G GAS, NATURAL GAS
GB GRADE BREAK
GV GATE VALVE
HDPE HIGH DENSITY POLYETHYLENE PIPE
HP HIGH POINT
IE, INV INVERT ELEVATION, INVERT
IR IRON ROD
IRR IRRIGATION
K RADIUS OF CURVATURE
LT LEFT
L LENGTH
LAT LATERAL SERVICE
LBS POUNDS
LD LAND DRAIN
LF LINEAR FEET
LG LIP OF GUTTER
LP LOW POINT
MAX, MIN MAXIMUM, MINIMUM
MH MANHOLE
MON MONUMENT
MJ MECHANICAL JOINT
NG NATURAL GRADE
NTS NOT TO SCALE
OC ON CENTER
OHP OVERHEAD POWER
PC POINT OF CURVATURE
PCC PORTLAND CEMENT CONCRETE
PI POINT OF INTERSECTION
PL PROPERTY LINE
PT POINT OF TANGENCY
PUE PUBLIC UTILITY EASEMENT
PVC POLYVINYL CHLORIDE
PVI POINT OF VERTICAL INFLECTION
RCP REINFORCED CONCRETE PIPE
R RADIUS
RC REBAR & CAP
RCL ROADWAY CENTERLINE
RCP REINFORCED CONCRETE PIPE
ROW RIGHT OF WAY
SEC SECTION
SD STORM DRAIN
SDCB STORM DRAIN CATCH BASIN
SDCO STORM DRAIN CLEANOUT
SLB&M SALT LAKE BASE & MERIDIAN SPECIFICATION
SS SANITARY SEWER
SSCO SANITARY SEWER CLEANOUT
STA STATION
STD STANDARD
SW SECONDARY WATER
TAN TANGENT
TB THRUST BLOCK
TBA TO BE ABANDONED
TBC TOP BACK CURB
TBW TOP BACK OF WALK
TC TOP OF CONCRETE
TCW TOP OF CURB WALL
TOA TOP OF ASPHALT
TOC TOP OF CONCRETE
TOE TOA OF SLOPE OR WALL
TOG TOP OF GRATE
TOW TOP OF WALL
TEL TELEPHONE
TYP TYPICAL
UTIL UTILITY
VC VERTICAL CURVE
W WATER
WM WATER METER

LEGEND

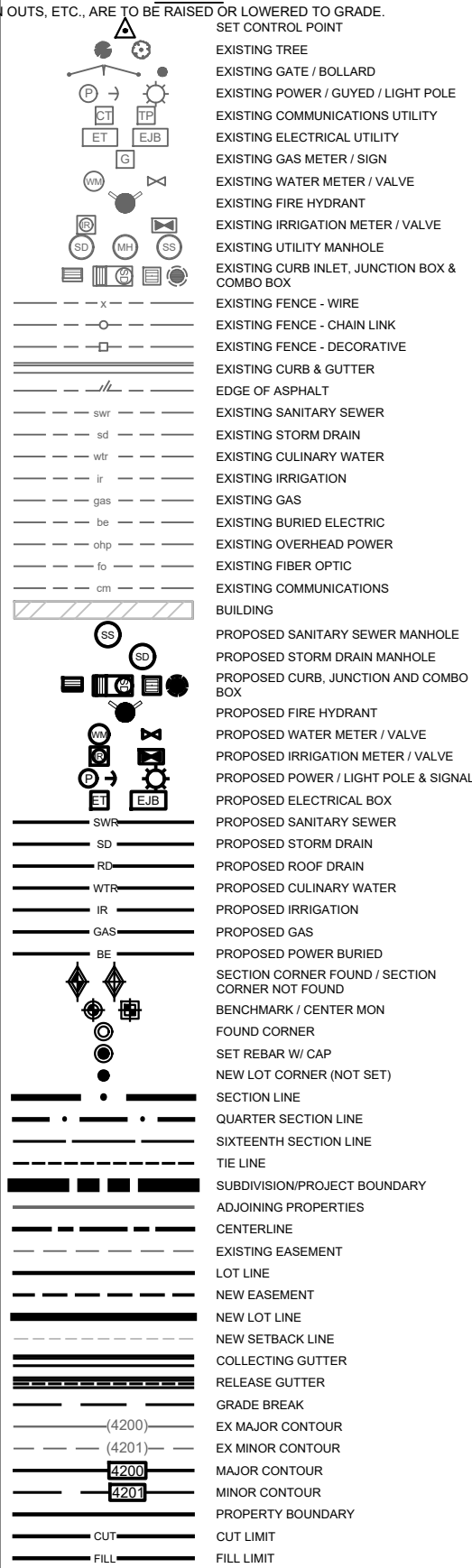
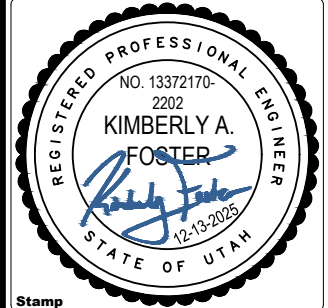


Table with 2 columns: REVISIONS, and 10 empty rows for recording changes.

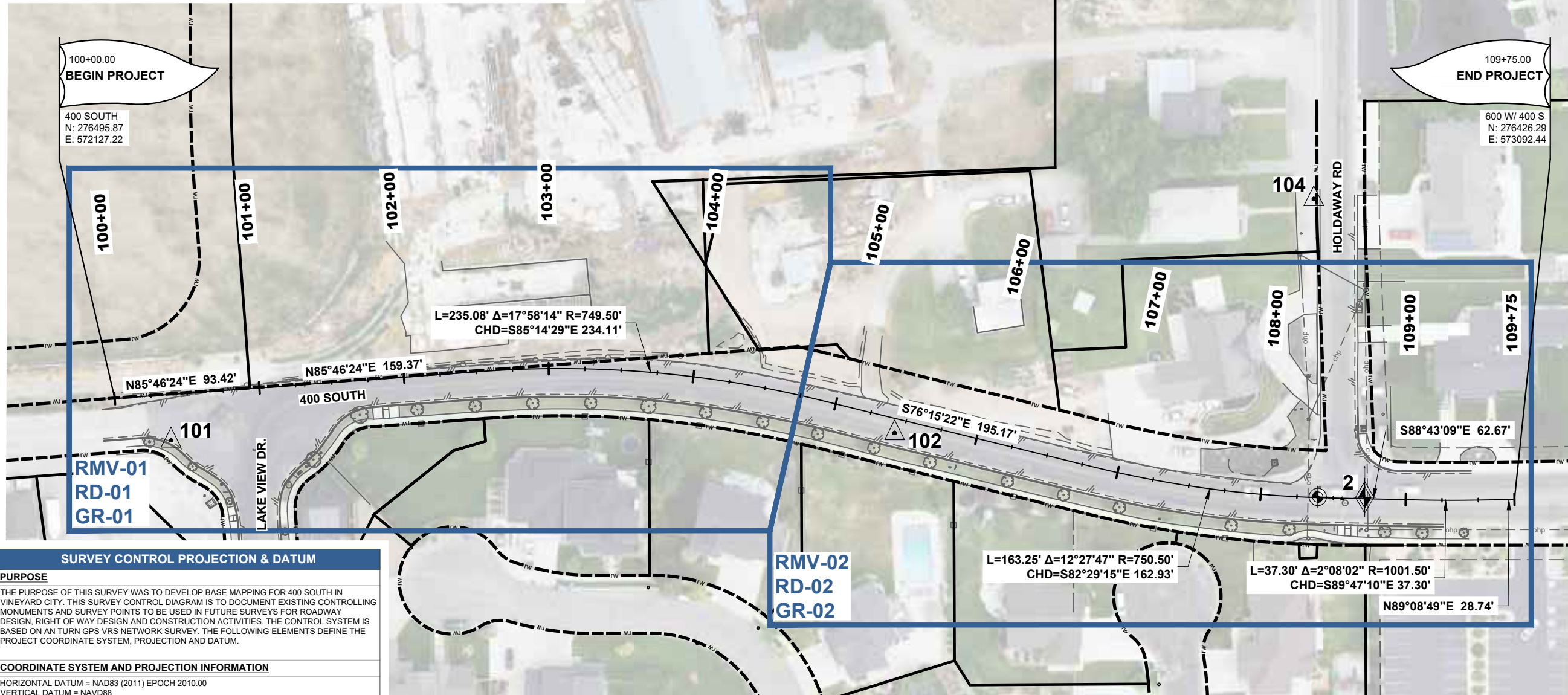
VINEYARD STAY CONNECTED logo. AVENUE CONSULTANTS logo. Address: Avenue Consultants, Inc. 588 W 400 S, Suite 250, Lindon, UT 84042. Website: www.avenueconsultants.com.



Stamp area containing: GENERAL NOTES AND ABBREVIATIONS, VINEYARD CITY, 400 S RECONSTRUCTION (HOLDAWAY RD TO LAKEVIEW DR), 125 SOUTH MAIN STREET, Designer KF, PE, Checker KC, PE, SHEET NO. 2 OF 18, SHEET TITLE G002.

SURVEY CONTROL TABLE

POINT ID	NORTHING	EASTING	ELEVATION	TYPE	SECTION & TOWNSHIP/RANGE	ADDRESS
1	276425.593	575732.876	4550.346	REF SW SEC. COR.	20, 6S 2E	400 S GENEVA RD
2	276427.759	572989.196	4536.917	N 1/4 CORNER	7, 6S 3E	400 S HOLDAWAY DR
101	276467.069	572166.040	4525.834	CP - MAG NAIL	--	400 S LAKE VIEW DR
102	276472.138	572665.055	4532.576	CP - MAG NAIL	--	367 S HOLDAWAY RD
103	276428.813	573188.596	4538.881	CP - MAG NAIL	--	435 W 3200 N
104	276633.566	572954.086	4537.495	CP - MAG NAIL	--	380 S HOLDAWAY RD
105	276878.256	572965.171	4538.427	CP - MAG NAIL	--	350 S HOLDAWAY RD



SURVEY CONTROL PROJECTION & DATUM

PURPOSE
 THE PURPOSE OF THIS SURVEY WAS TO DEVELOP BASE MAPPING FOR 400 SOUTH IN VINEYARD CITY. THIS SURVEY CONTROL DIAGRAM IS TO DOCUMENT EXISTING CONTROLLING MONUMENTS AND SURVEY POINTS TO BE USED IN FUTURE SURVEYS FOR ROADWAY DESIGN, RIGHT OF WAY DESIGN AND CONSTRUCTION ACTIVITIES. THE CONTROL SYSTEM IS BASED ON AN TURN GPS VRS NETWORK SURVEY. THE FOLLOWING ELEMENTS DEFINE THE PROJECT COORDINATE SYSTEM, PROJECTION AND DATUM.

COORDINATE SYSTEM AND PROJECTION INFORMATION
 HORIZONTAL DATUM = NAD83 (2011) EPOCH 2010.00
 VERTICAL DATUM = NAVD88
 COORDINATE SYSTEM = NAD 1983 UTAH CENTRAL ZONE 4302 PROJECTED AT GROUND
 GEOID MODEL = GEOID 18
 UNITS = US SURVEY FOOT
 FALSE NORTHING OFFSET = -7,000,000
 FALSE EASTING OFFSET = -1,000,000
 COMBINED SCALE FACTOR = 1.000283562

THE COMBINED SCALE FACTOR AND CONVERGENCE ANGLE FOR THIS PROJECT WERE DERIVED FROM THE FOLLOWING VALUES:
 PROJECT LATITUDE = N 40°17'23.13478"
 PROJECT LONGITUDE = W 111°44'35.92556"
 PROJECT HEIGHT = 4480.348'

PROJECT DATUM (US SURVEY FOOT):
 PROJECT COORDINATES ARE MODIFIED NAD 1983 UTAH CENTRAL ZONE 4302 COORDINATES. THE STATE PLANE COORDINATE VALUES ARE SCALED BY THE COMBINED SCALE FACTOR (FROM COORDINATE 0.0 OR LATITUDE 20°31'27.66067" N, LONGITUDE 116°02'57.11642" W) THEN TRUNCATED BY THE FALSE NORTHING AND EASTING OFFSET VALUES. THE RESULTING PROJECT COORDINATES ARE TABULATED ON SHEET SC-03 AND SC-04.

TO CONVERT PROJECT COORDINATES TO STATE PLANE COORDINATES:
 ((PROJECT NORTHING + 7,000,000) / 1.000283562) = STATE PLANE NORTHING
 ((PROJECT EASTING + 1,000,000) / 1.000283562) = STATE PLANE EASTING

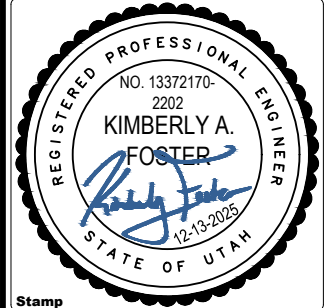
- NOTES**
- SURVEY NOTES**
- DIGITAL LEVELLING WAS NOT PERFORMED AS PART OF THIS CONTROL SURVEY.
 - THIS CONTROL SURVEY CONFORMS TO THE UDOT SURVEY AND GEOMATICS STANDARDS 2017.
 - ALL BEARINGS SHOWN, OR COMPUTED FROM COORDINATE VALUES TABULATED HEREIN, ARE GRID BEARINGS OF THE NAD 1983 UTAH CENTRAL ZONE 4302.
 - SURVEY CONTROL POINTS MEET OR EXCEED THE ACCURACY REQUIREMENTS OF UDOT HORIZONTAL PROJECT CONTROL AT 2CM (0.07") LOCAL POSITIONAL ACCURACY WITH 95% CERTAINTY.
 - CONTROL POINT MARKS ARE SUBJECT TO THE EFFECTS OF SURFACE ACTIVITY AND SEASONAL MOVEMENTS. MARKS MAY ALSO BE DISTURBED, DAMAGED OR REMOVED AFTER THE COMPLETION OF THIS SURVEY. CONTROL POINT POSITIONS SHOULD BE FIELD VERIFIED PRIOR TO SUBSEQUENT SURVEY ACTIVITIES. THIS CONTROL SURVEY WAS COMPLETED IN AUGUST 2023.

REVISIONS:

VINEYARD
 BEST PRACTICES

avenue CONSULTANTS
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SURVEY CONTROL
 VINEYARD CITY

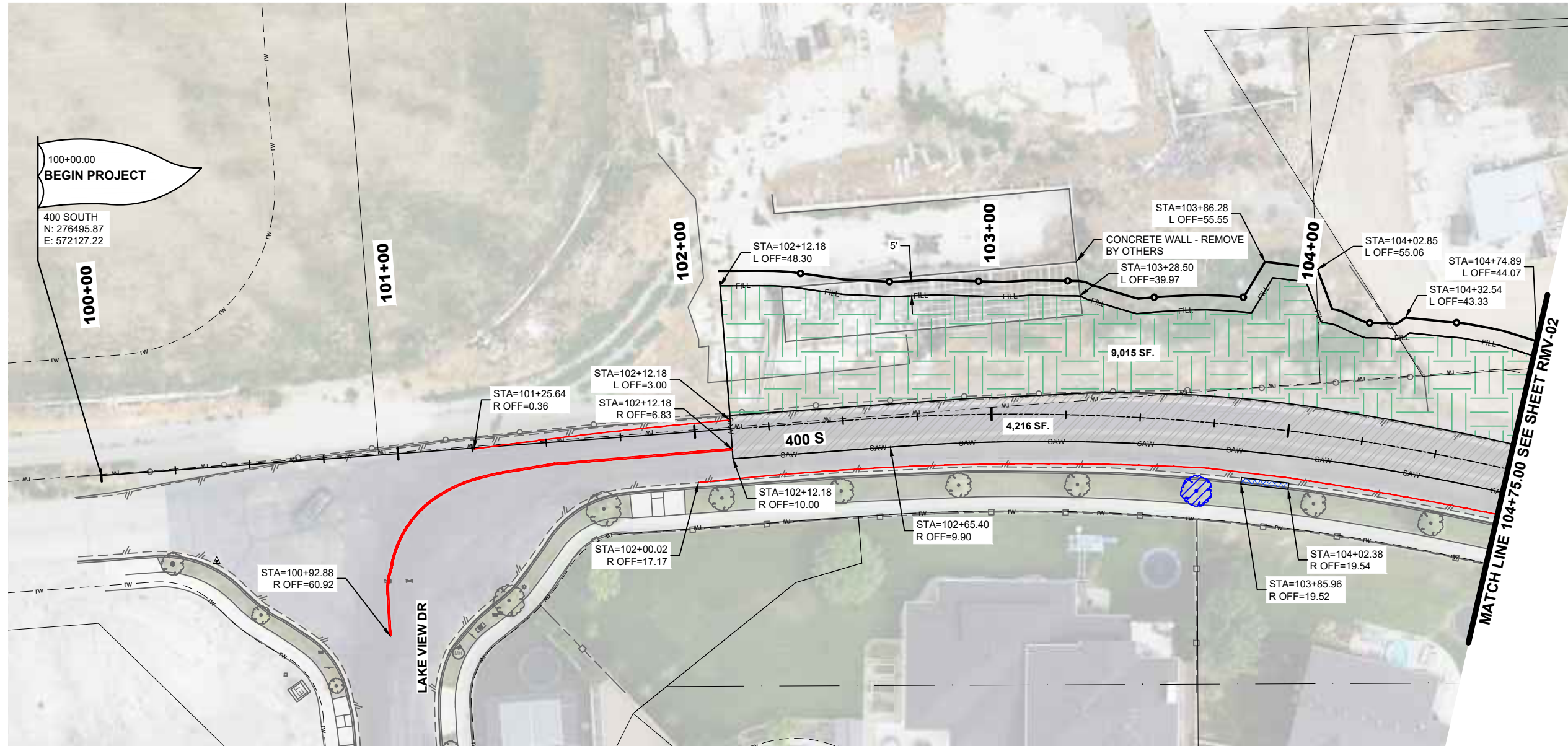
**400 S RECONSTRUCTION
 (HOLDAWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

SHEET NO. 3 OF 18

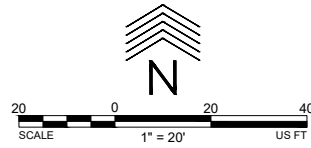
SHEET TITLE **CS100**



100+00.00
BEGIN PROJECT
 400 SOUTH
 N: 276495.87
 E: 572127.22

REMOVAL LEGEND	
	CLEARING AND GRUBBING STRIP AND STOCKPILE TOPSOIL (6")
	REMOVE AND DISPOSE ASPHALT PAVEMENT SECTION
	TEMPORARY SILT FENCE
	REMOVE CONCRETE CURB AND GUTTER
	REMOVE CONCRETE SIDEWALK
	REMOVE EXISTING STRIPING / PAINT MESSAGE
	SALVAGE AND STOCKPILE EXISTING GRAVEL
	REMOVE AND DISPOSE EXISTING TREE

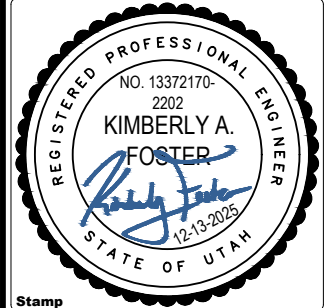
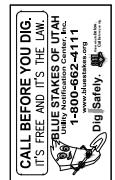
SHEET NOTES	
1.	CONTRACTOR TO FIELD LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.
2.	PROTECT IN PLACE EXISTING UTILITIES.
3.	SEE RD SERIES FOR SIGN REMOVALS AND RELOCATIONS.
4.	PROVIDE TEMPORARY SILT FENCES DURING DEMOLITION AND CONSTRUCTION PHASES PER CITY STANDARDS AND SPECIFICATIONS.



NO.	REVISIONS:



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Stamp

REMOVAL
 VINEYARD CITY

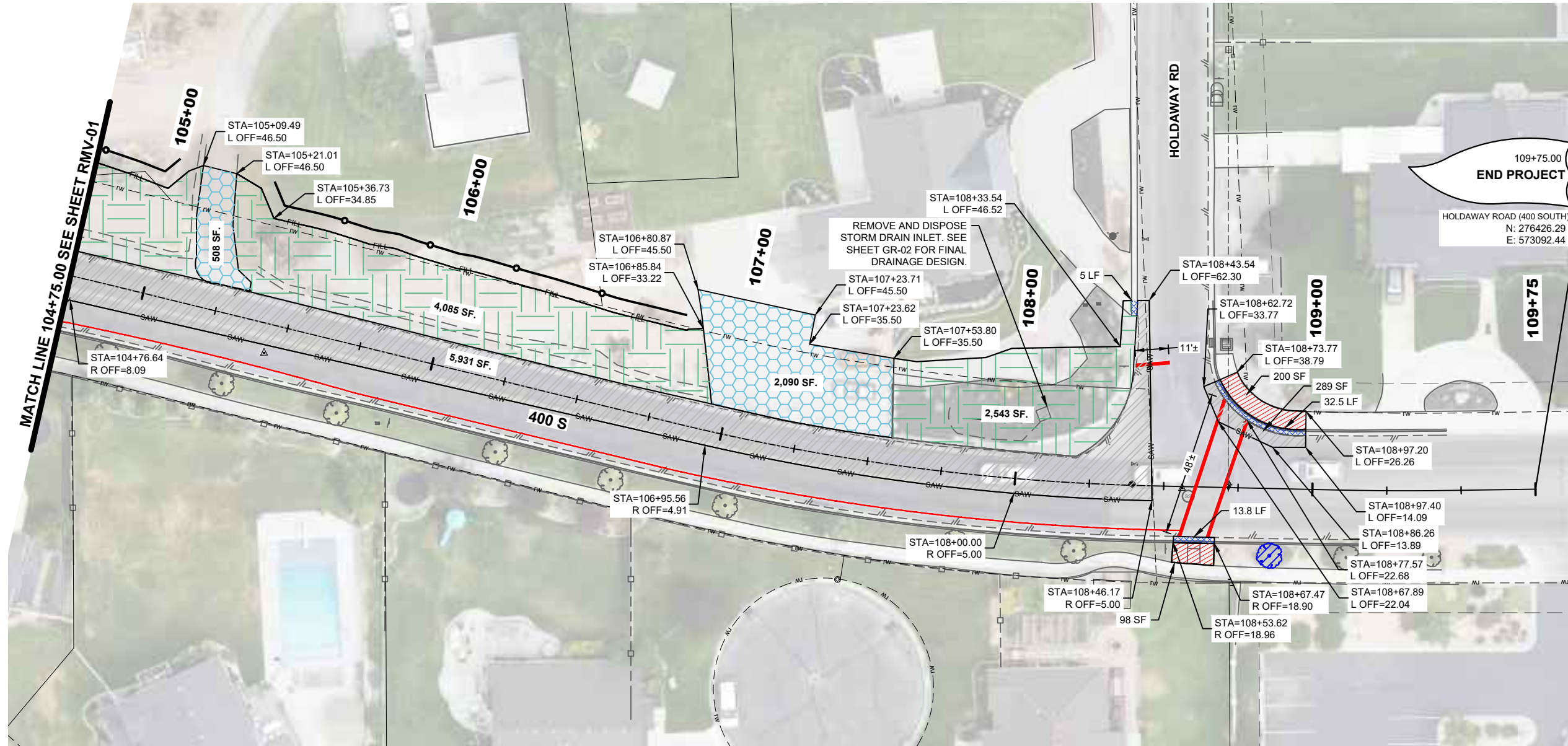
**400 S RECONSTRUCTION
 (HOLDAWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

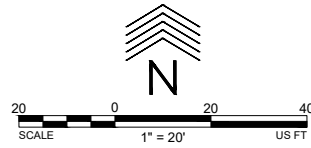
SHEET NO. 4 OF 18

SHEET TITLE **RMV-01**



REMOVAL LEGEND	
	CLEARING AND GRUBBING STRIP AND STOCKPILE TOPSOIL (6")
	REMOVE AND DISPOSE ASPHALT PAVEMENT SECTION
	TEMPORARY SILT FENCE
	REMOVE CONCRETE CURB AND GUTTER
	REMOVE CONCRETE SIDEWALK
	REMOVE EXISTING STRIPING / PAINT MESSAGE
	SALVAGE AND STOCKPILE EXISTING GRAVEL
	REMOVE AND DISPOSE EXISTING TREE

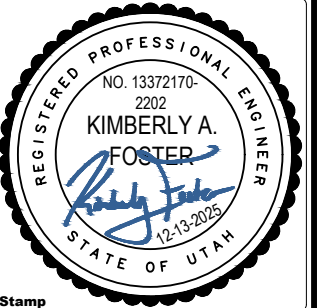
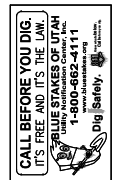
SHEET NOTES	
1.	CONTRACTOR TO FIELD LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.
2.	PROTECT IN PLACE EXISTING UTILITIES.
3.	SEE RD SERIES FOR SIGN REMOVALS AND RELOCATIONS.
4.	PROVIDE TEMPORARY SILT FENCES DURING DEMOLITION AND CONSTRUCTION PHASES PER CITY STANDARDS AND SPECIFICATIONS.



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REMOVAL
 VINEYARD CITY

**400 S RECONSTRUCTION
 (HOLDAWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

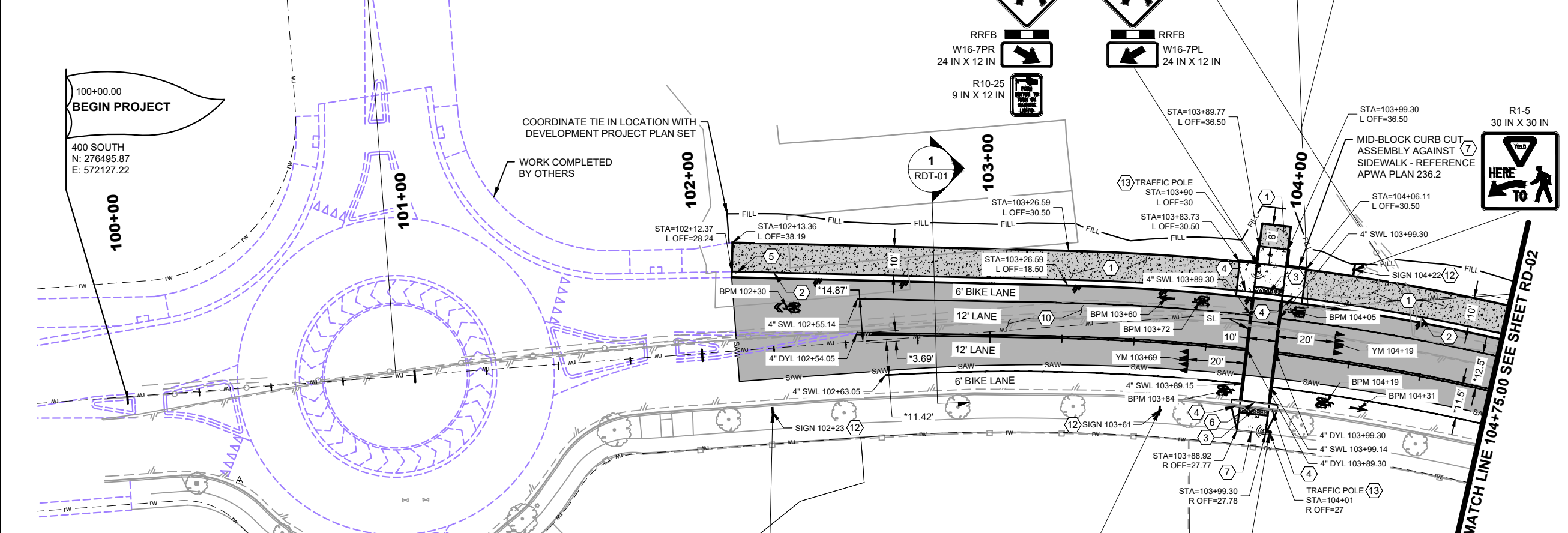
Designer KF, PE Checker KC, PE

SHEET NO. 5 OF 18

SHEET TITLE **RMV-02**

RRFB ASSEMBLY MATERIALS SCHEDULE							
EA	EA	EA	EA	EA	EA	EA	EA
2	8	4	2	2	2	2	2

SHEET NOTES	
1.	COLOR DASHED LINEWORK INDICATES WORK DONE BY OTHERS. CONTRACTOR TO COORDINATE WITH DEVELOPMENT PLANS DONE BY OTHERS FOR TIE IN.
2.	PROTECT IN PLACE ALL EXISTING UTILITIES UNLESS OTHERWISE NOTED.
3.	SEE SURVEY CONTROL SHEET, CS100, FOR ALIGNMENT INFORMATION.
4.	* DIMENSION INDICATES STRIPE TO CENTERLINE OF ALIGNMENT.



SITE KEYED NOTES	
1	CONCRETE SIDEWALK SECTION PER VINEYARD STD DWG NO. 4 - SEE PLAN FOR WIDTH.
2	2' TYPICAL CURB & GUTTER PER VINEYARD STD DWG NO. 5
3	TYPE A - 24" CURB & GUTTER ADA ACCESS PER APWA PLAN 205.1
4	TRANSITION BETWEEN STANDARD CURB & GUTTER AND ADA ACCESS CURB AND GUTTER.
5	CURB AND GUTTER 5' TRANSITION TO MATCH EXISTING
6	CURB AND GUTTER INSTALLATION WITH NO ASPHALT CUT PER APWA PLAN 252
7	PERPENDICULAR PEDESTRIAN MID BLOCK RAMP PER VINEYARD STD DWG NO. 7B
8	CORNER PEDESTRIAN ACCESS RAMP PER VINEYARD STD DWG NO. 7A
9	CONCRETE DIP DRIVEWAY APPROACH PER APWA PLAN NO 215
10	ASPHALT PAVEMENT PER VINEYARD STANDARDS AND SPECIFICATIONS. MATCH EXISTING ASPHALT PAVEMENT AND BASE COURSE DEPTHS.
11	LANDSCAPE RESTORATION - COORDINATE WITH CITY AND OWNER FOR MATERIALS AND SPECIFICATIONS
12	SIGN, POLE, & BASE INSTALLATION PER VINEYARD STD DWG NO. 25
13	15' STATE FURNISHED TRAFFIC POLE, CONTRACTOR FURNISHED PED POLE FOUNDATION - 2'X3', AND CONTRACTOR FURNISHED PUSH BUTTON SIGN ASSEMBLY. INSTALL STATE FURNISHED SOLAR PANEL AND LIGHT ON TOP OF POLE.
14	SALVAGED GRAVEL NEW LOCATION
15	CORNER CURB CUT ASSEMBLY W/ CURB WALL PER APWA PLAN 235.1 & 238

SIGNING LEGEND	
	NEW SIGN
	RELOCATE SIGN
	PROTECT IN PLACE SIGN

SIGN SCHEDULE		
SIGN TYPE	SIGN SIZE	QTY
W11-2	30 IN X 30 IN	4
W16-7PL	24 IN X 12 IN	2
R10-25	9 IN X 12 IN	2
R1-5	30 IN X 30 IN	2
R7-9a	12 IN X 18 IN	1

STRIPING KEY	
SWL	SOLID WHITE LINE - PAINT
DYL	DOUBLE YELLOW LINE - PAINT
SL	12" STOP LINE - THERMOPLASTIC
YM	YIELD MARKING - THERMOPLASTIC
BPM	BIKE PAVEMENT MESSAGE - THERMOPLASTIC

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REGISTERED PROFESSIONAL ENGINEER

NO. 13372170-2202

KIMBERLY A. FOSTER

12-13-2025

STATE OF UTAH

ROADWAY

VINEYARD CITY

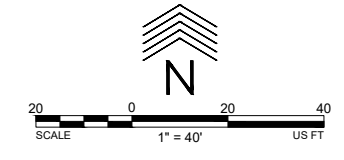
400 S RECONSTRUCTION (HOLDADWAY RD TO LAKEVIEW DR)

125 SOUTH MAIN STREET

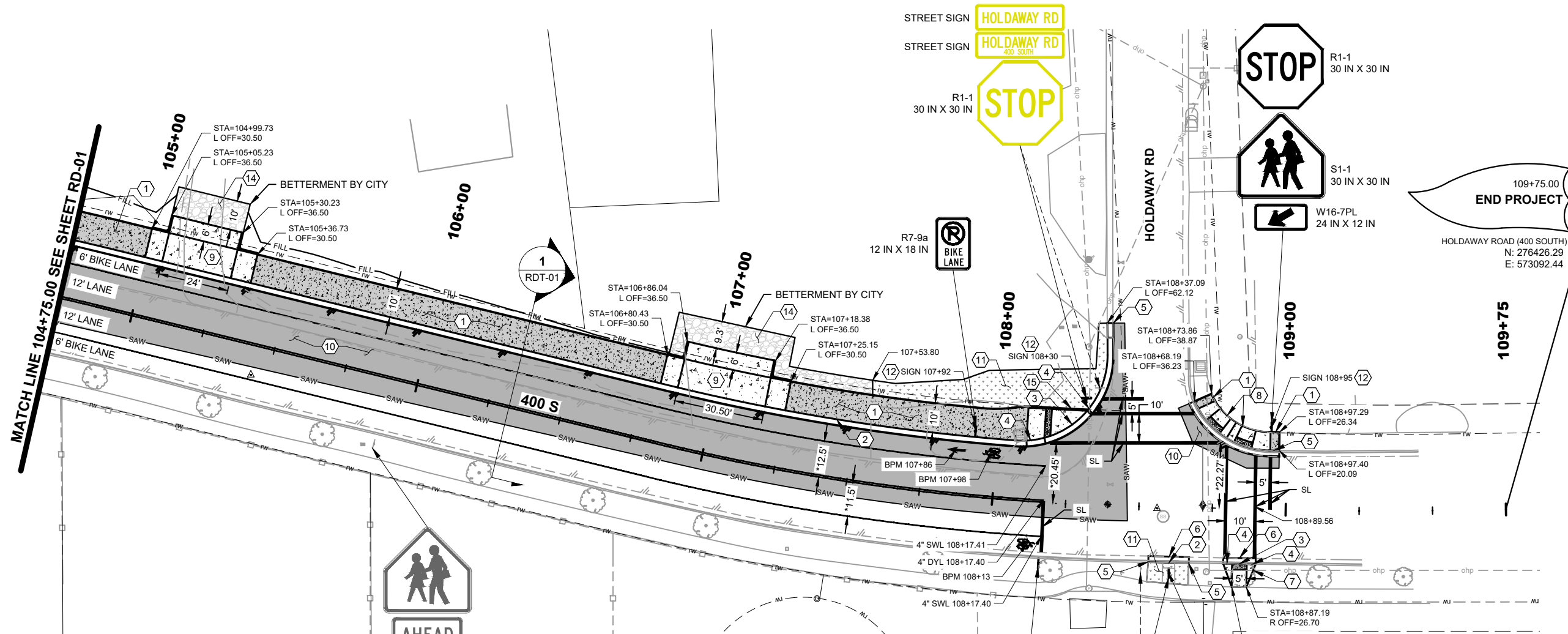
Designer KF, PE Checker KC, PE

SHEET NO. 6 OF 18

SHEET TITLE **RD-01**



SHEET NOTES	
1.	COLOR DASHED LINEWORK INDICATES WORK DONE BY OTHERS. CONTRACTOR TO COORDINATE WITH DEVELOPMENT PLANS DONE BY OTHERS FOR TIE IN.
2.	PROTECT IN PLACE ALL EXISTING UTILITIES UNLESS OTHERWISE NOTED.
3.	SEE SURVEY CONTROL SHEET, CS100, FOR ALIGNMENT INFORMATION.
4.	* DIMENSION INDICATES STRIPE TO CENTERLINE OF ALIGNMENT.

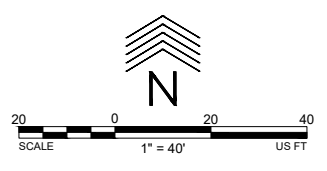


SITE KEYED NOTES	
1	CONCRETE SIDEWALK SECTION PER VINEYARD STD DWG NO. 4
2	2' TYPICAL CURB & GUTTER PER VINEYARD STD DWG NO. 5
3	TYPE A - 24" CURB & GUTTER ADA ACCESS PER APWA PLAN 205.1
4	TRANSITION BETWEEN STANDARD CURB & GUTTER AND ADA ACCESS CURB AND GUTTER.
5	CURB AND GUTTER 5' TRANSITION TO MATCH EXISTING
6	CURB AND GUTTER INSTALLATION WITH NO ASPHALT CUT PER APWA PLAN 252
7	PERPENDICULAR PEDESTRIAN MID BLOCK RAMP PER VINEYARD STD DWG NO. 7B
8	CORNER PEDESTRIAN ACCESS RAMP PER VINEYARD STD DWG NO. 7A
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12	SIGN, POLE, & BASE INSTALLATION PER VINEYARD STD DWG NO. 25
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14	SALVAGED GRAVEL NEW LOCATION
15	CORNER CURB CUT ASSEMBLY W/ CURB WALL PER APWA PLAN 235.1 & 238

SIGNING LEGEND	
	NEW SIGN
	RELOCATE SIGN
	PROTECT IN PLACE SIGN

SIGN SCHEDULE			
SIGN TYPE	SIGN SIZE	QTY	
R1-1	30 IN X 30 IN	2	
R7-9a	12 IN X 18 IN	1	
S1-1	30 IN X 30 IN	1	
W16-7PL	24 IN X 12 IN	1	
RELOCATE - STOP (1) & STREET (2)			
RELOCATE - CROSSWALK (1) & STREET (1)			

STRIPING KEY	
SWL	SOLID WHITE LINE - PAINT
DYL	DOUBLE YELLOW LINE - PAINT
SL	12" STOP LINE - THERMOPLASTIC
YM	YIELD MARKING - THERMOPLASTIC
BPM	BIKE PAVEMENT MESSAGE - THERMOPLASTIC



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KIMBERLY A. FOSTER
12-13-2025
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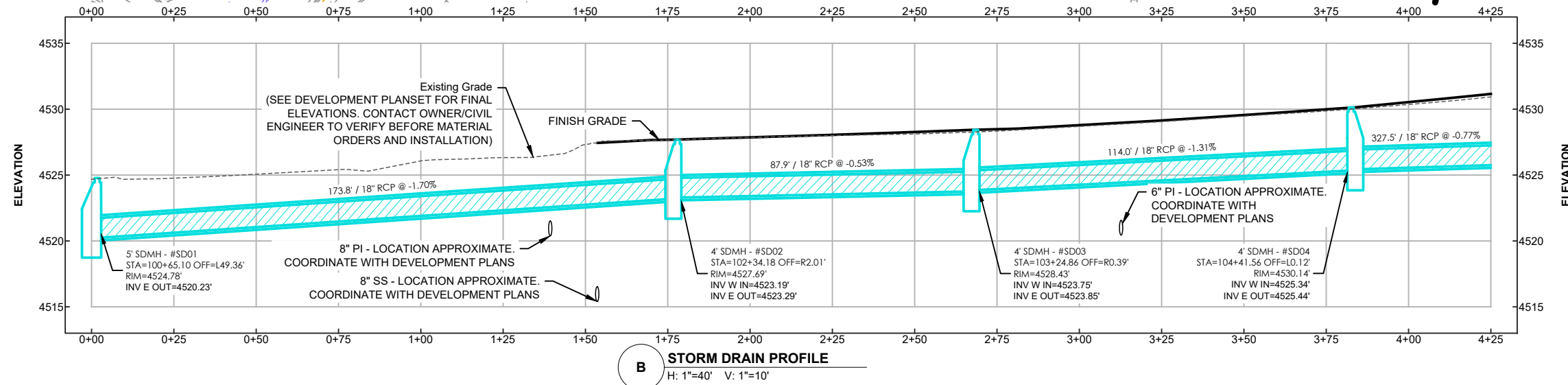
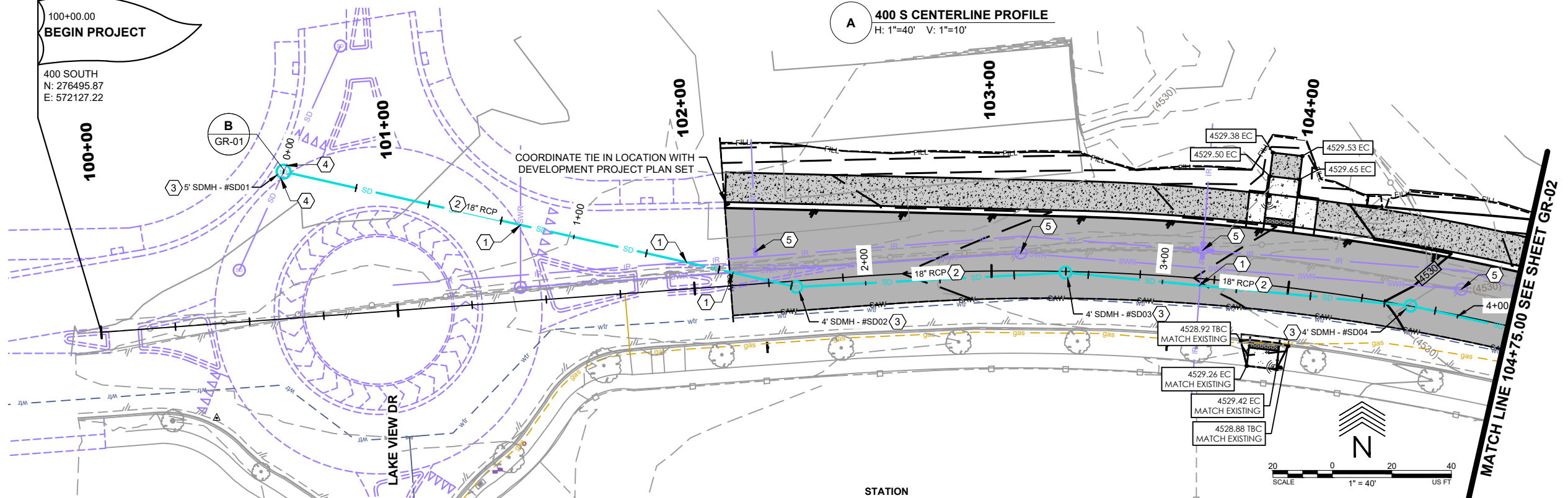
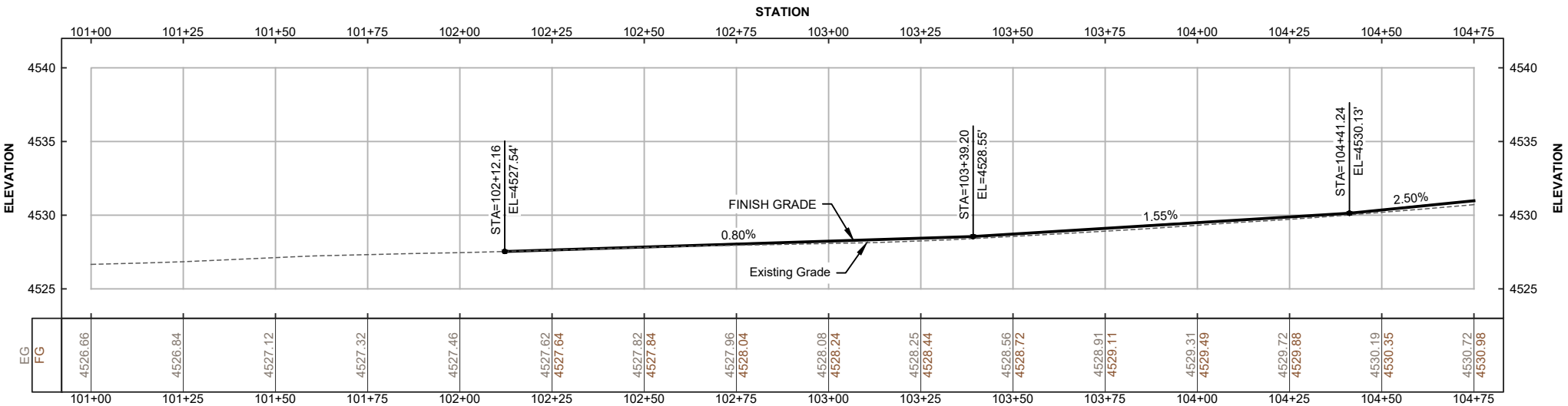
**400 S RECONSTRUCTION
(HOLDAWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

SHEET NO. 7 OF 18
SHEET TITLE **RD-02**

- KEYED NOTES**
- 1 PROVIDE 1' VERTICAL CLEARANCE BETWEEN OUTSIDE OF NEW PIPE TO EXISTING PIPE
 - 2 STORM DRAIN PIPE WITH TYPICAL TRENCH PER VINEYARD STD DWG 13
 - 3 STORM DRAIN JUNCTION BOX MANHOLE WITH CONCRETE COLLAR PER VINEYARD STD DWG 9A - SEE PLAN FOR SIZE
 - 4 CONNECT TO EXISTING STORM DRAIN PIPE PER VINEYARD STANDARDS AND SPECIFICATIONS
 - 5 RAISE/LOWER UTILITY STRUCTURE TO FINISH GRADE. CONCRETE COLLAR INCLUDED IN DEVELOPMENT PLANS.
 - 6 RAISE/LOWER EXISTING WATER VALVE OR SURVEY MONUMENT TO FINISH GRADE WITH CONCRETE COLLAR PER VINEYARD STD DWG 17
- SHEET NOTES**
1. THE ENGINEER HAS MADE AN EXTENSIVE EFFORT TO LOCATED ALL UTILITY LINES. UTILITIES SHOWN ON PLANS ARE FROM RECORD MAPS PROVIDED BY OTHERS AND OBSERVATIONS IN THE FIELD. CONTRACTOR TO VERIFY HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES PRIOR TO BEGINNING WORK.
 2. PROTECT IN PLACE ALL EXISTING UTILITIES UNLESS OTHERWISE NOTED.
 3. COORDINATE WITH DEVELOPMENT PROJECT AND PLANS FOR STORM DRAIN AND GRADING TIE IN ELEVATIONS AND LOCATIONS. DEVELOPMENT PROJECT PROPOSED UTILITIES SHOWN IN PURPLE ON THIS PLAN. ACTUAL LOCATION MAY VARY.



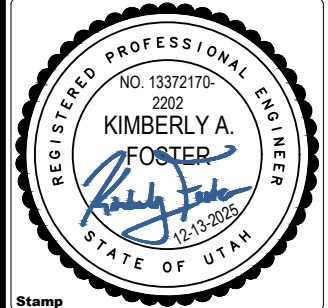
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GRADING & DRAINAGE

VINEYARD CITY

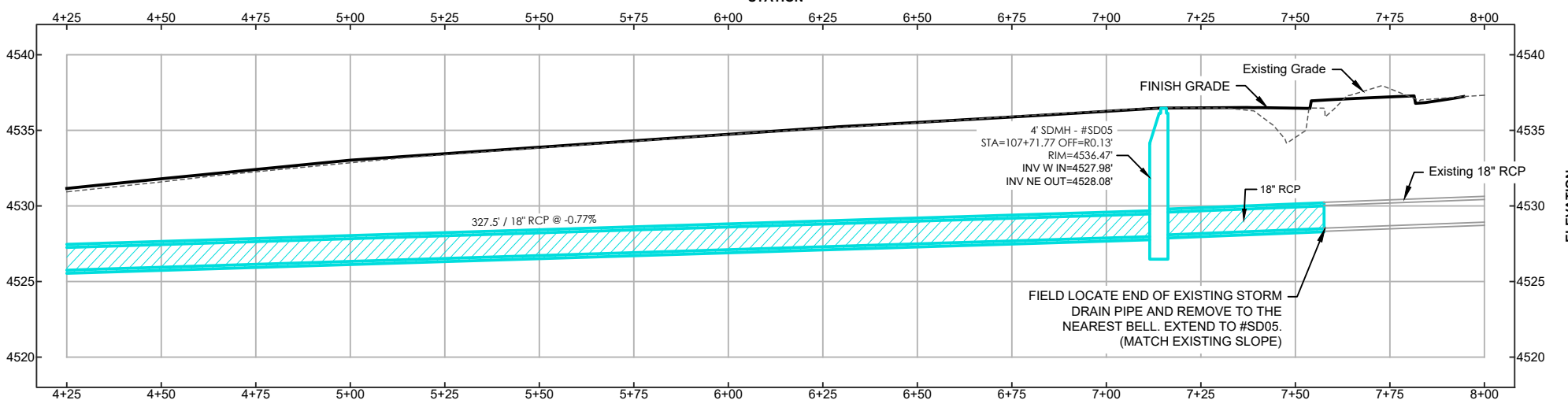
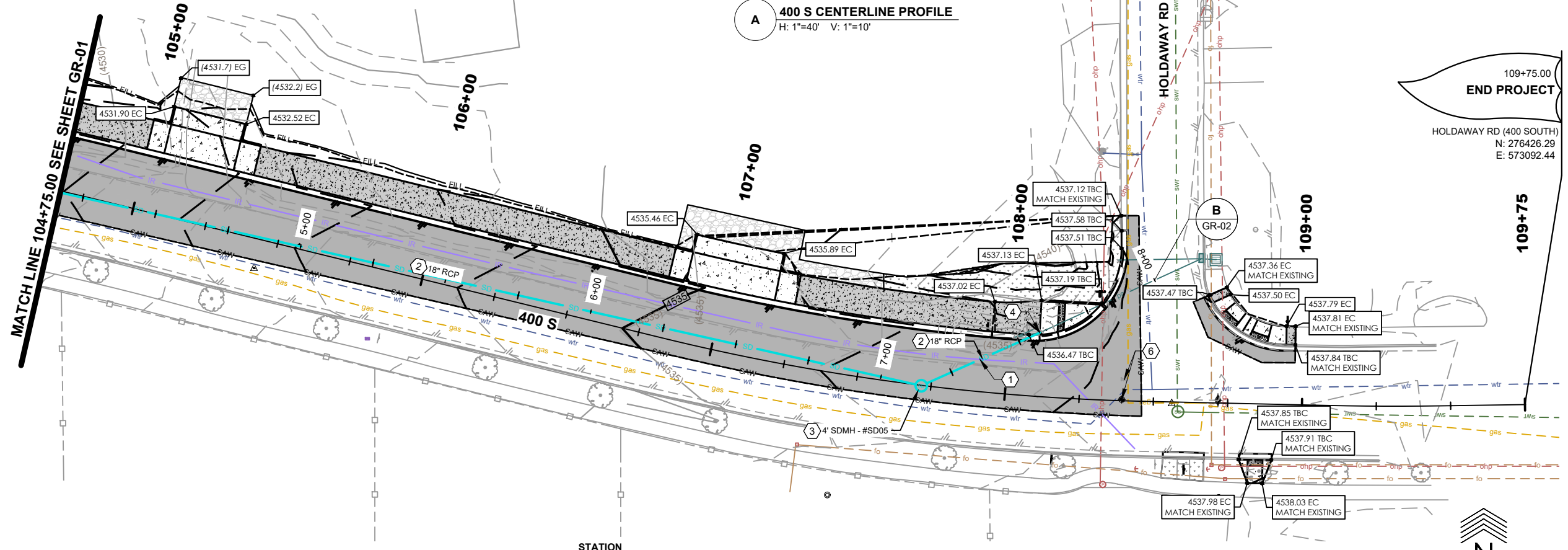
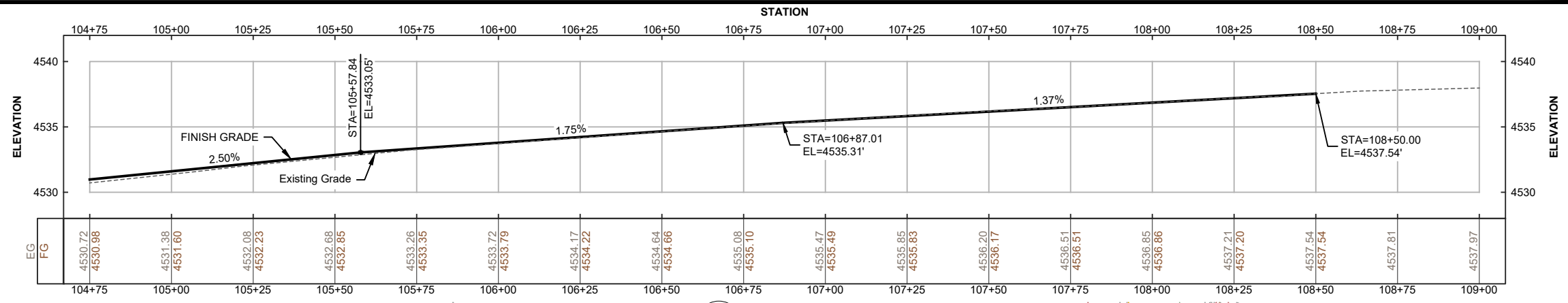
**400 S RECONSTRUCTION
(HOLDWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

SHEET NO. 8 OF 18

SHEET TITLE **GR-01**



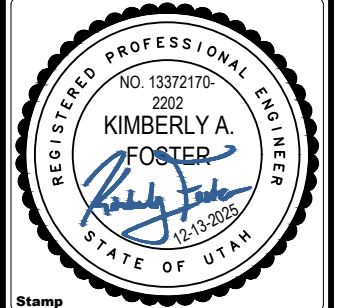
- KEYED NOTES**
- PROVIDE 1' VERTICAL CLEARANCE BETWEEN OUTSIDE OF NEW PIPE TO EXISTING PIPE
 - STORM DRAIN PIPE WITH TYPICAL TRENCH PER VINEYARD STD DWG 13
 - STORM DRAIN JUNCTION BOX MANHOLE WITH CONCRETE COLLAR PER VINEYARD STD DWG 9A - SEE PLAN FOR SIZE
 - CONNECT TO EXISTING STORM DRAIN PIPE PER VINEYARD STANDARDS AND SPECIFICATIONS
 - RAISE/LOWER UTILITY STRUCTURE TO FINISH GRADE. CONCRETE COLLAR INCLUDED IN DEVELOPMENT PLANS.
 - RAISE/LOWER EXISTING WATER VALVE OR SURVEY MONUMENT TO FINISH GRADE WITH CONCRETE COLLAR PER VINEYARD STD DWG 17
- SHEET NOTES**
- THE ENGINEER HAS MADE AN EXTENSIVE EFFORT TO LOCATED ALL UTILITY LINES. UTILITIES SHOWN ON PLANS ARE FROM RECORD MAPS PROVIDED BY OTHERS AND OBSERVATIONS IN THE FIELD. CONTRACTOR TO VERIFY HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES PRIOR TO BEGINNING WORK.
- COORDINATE WITH DEVELOPMENT PROJECT AND PLANS FOR STORM DRAIN AND GRADING TIE IN ELEVATIONS AND LOCATIONS.
 - DEVELOPMENT PROJECT PROPOSED UTILITIES SHOWN IN PURPLE ON THIS PLAN. ACTUAL LOCATION MAY VARY.

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GRADING & DRAINAGE

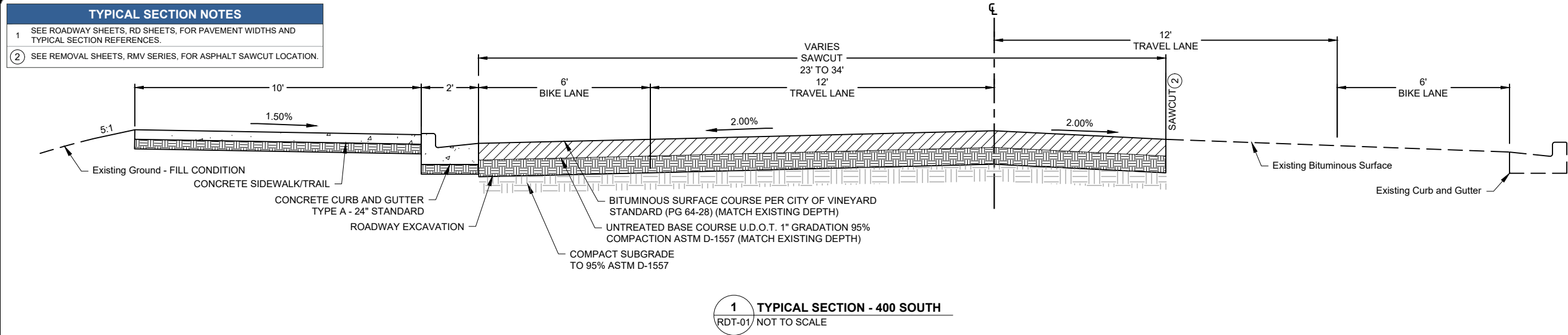
400 S RECONSTRUCTION (HOLDADWAY RD TO LAKEVIEW DR)

125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

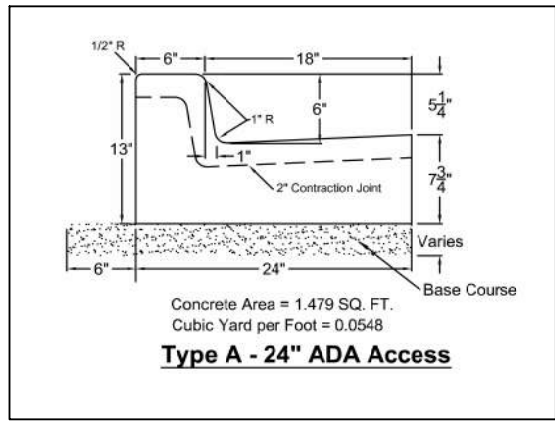
SHEET NO. 9 OF 18

SHEET TITLE **GR-02**



Curb and gutter

- GENERAL**
 - Variance from specified dimensions and slopes must be acceptable to the ENGINEER. System configuration may be changed at ENGINEER's discretion.
 - Additional requirements are specified in APWA Section 32 16 13.
- PRODUCTS**
 - Base Course: Untreated base course, APWA Section 32 11 23. Do not use gravel as a base course without ENGINEER's permission.
 - Expansion Joint Filler: 1/2-inch thick type F1 full depth, APWA Section 32 13 73.
 - Concrete: Class 4000, APWA Section 03 30 04. If necessary, provide concrete that achieves design strength in less than 7 days. Use caution; however, as concrete crazing (spider cracks) may develop if air temperature exceeds 90 degrees F.
 - Concrete Curing Agent: Clear membrane forming compound with fugitive dye (Type ID Class A), APWA Section 03 39 00.
- EXECUTION**
 - Base Course Placement: APWA Section 32 05 10. Thickness is 6-inches if flow-line grade is 0.5 percent (s=0.005) or greater. If slope is less, provide 8-inches. Maximum lift thickness before compaction is 8-inches when using riding equipment or 6-inches when using hand held equipment. Compaction is 95 percent or greater relative to a modified proctor density, APWA Section 31 23 26.
 - Concrete Placement: APWA Section 03 30 10.
 - Install expansion joints vertical, full depth, with top of filler set flush with concrete surface. Install at the start or end of a street intersection curb return. Expansion joints are not required in concrete placement using slip-form construction.
 - Install contraction joints vertical, 1/8-inch wide or 1/4 slab thickness if the slab is greater than 8-inches thick. Match joint location in adjacent Portland-cement concrete roadway pavement.
 - Provide 1/2-inch radius edges. Apply a broom finish. Apply a curing agent.
 - Protection and Repair: Protect concrete from deicing chemicals during cure. Repair construction that does not drain. If necessary, fill flow-line with water to verify.

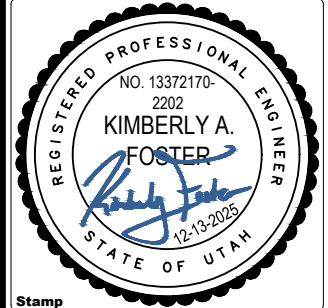


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ROADWAY DETAILS
VINEYARD CITY

**400 S RECONSTRUCTION
(HOLDAWAY RD TO LAKEVIEW DR)**
125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

SHEET NO. 10 OF 18
SHEET TITLE **RDT-01**

Dip driveway approach

1. GENERAL

- A. Variance from specified dimensions and slopes must be acceptable to the ENGINEER. System configuration may be changed at ENGINEER's discretion.
- B. Additional requirements are specified in APWA Section 32 16 13.

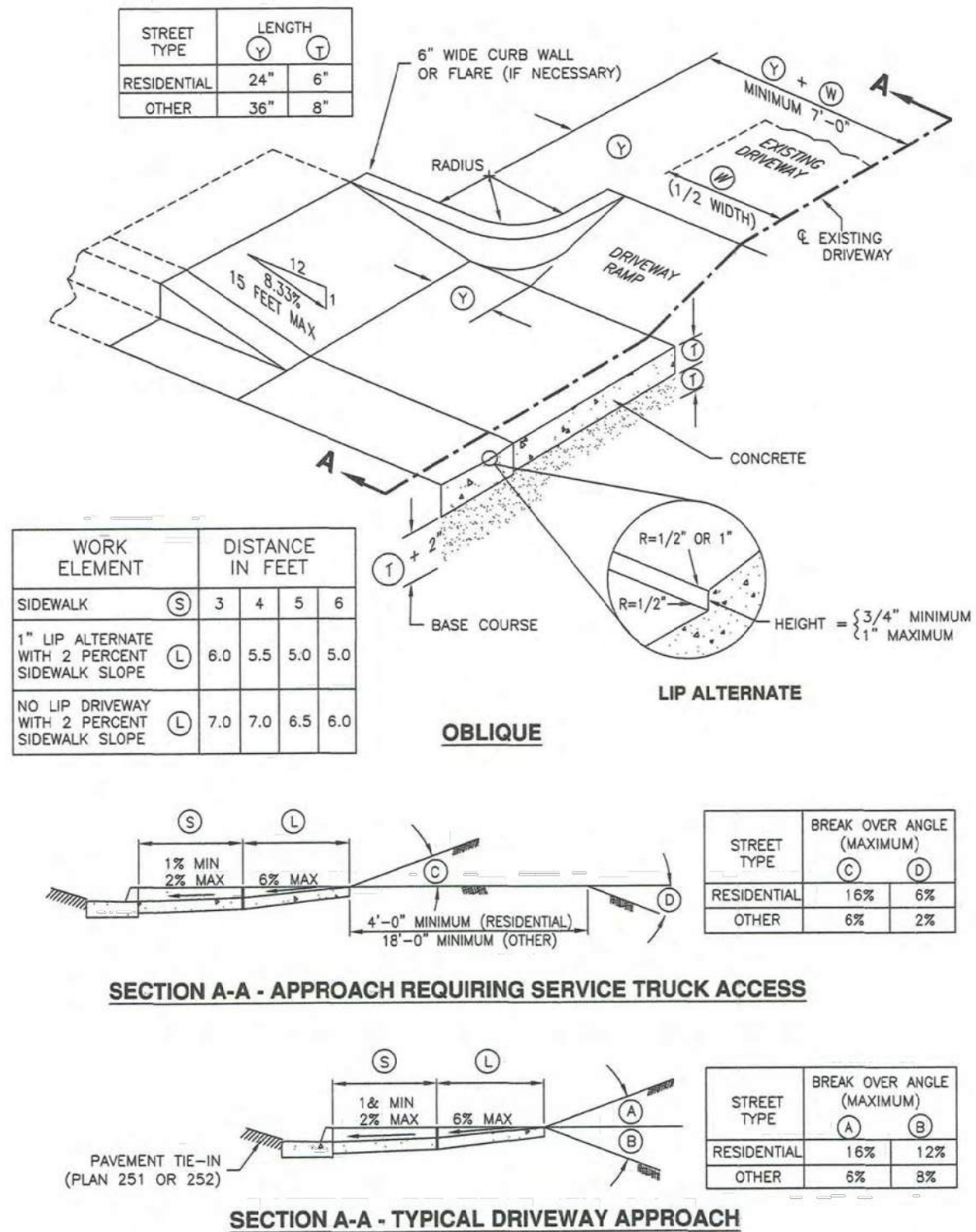
2. PRODUCTS

- A. Base Course: Untreated base course, APWA Section 32 11 23. Do not use gravel as a base course without ENGINEER's permission.
- B. Expansion Joint Filler: 1/2-inch thick type F1 full depth, APWA Section 32 13 73..
- C. Concrete: Class 4000, APWA Section 03 30 04. If necessary, provide concrete that achieves design strength in less than 7 days. Use caution; however, as concrete crazing (spider cracks) may develop if air temperature exceeds 90 degrees F.
- D. Reinforcement: Galvanized or epoxy coated, deformed, 60 ksi yield grade steel, ASTM A615.
- E. Concrete Curing Agent: Clear membrane forming compound with fugitive dye (Type ID Class A), APWA Section 03 39 00.

3. EXECUTION

- A. Base Course Placement: APWA Section 32 05 10. Maximum lift thickness before compaction is 8-inches when using riding equipment or 6-inches when using hand held equipment. Compaction is 95 percent or greater relative to a modified proctor density, APWA Section 31 23 26.
- B. Concrete Placement: APWA Section 03 30 10.
 - 1) Install expansion joints vertical, full depth, with top of filler set flush with concrete surface.
 - 2) Install contraction joints vertical, 1/8-inch wide or 1/4 slab thickness if the slab is greater than 8-inches thick. Maximum length to width ratio for non-square panels is 1.5 to 1. Maximum panel length (in feet) is 1.5 times the slab thickness (in inches).
 - 3) Provide 1/2-inch radius edges. Apply a broom finish. Apply a curing agent.
- C. Protection and Repair. Protect concrete from deicing chemicals during cure. Repair construction that does not drain. If necessary, fill flow-line with water to verify.

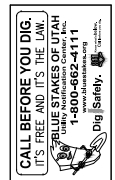
215



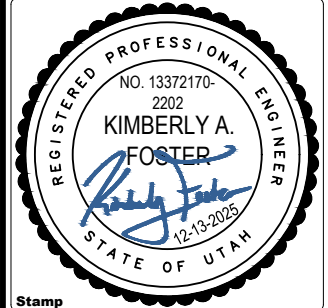
Dip driveway approach

Plan
215
December 2009

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ROADWAY DETAILS
VINEYARD CITY
400 S RECONSTRUCTION (HOLDADWAY RD TO LAKEVIEW DR)
125 SOUTH MAIN STREET

Designer KF, PE	Checker KC, PE
SHEET NO. 11 OF 18	
SHEET TITLE	RDT-02

Corner curb cut assembly

1. GENERAL

- A. Where existing elements or spaces are altered to receive an assembly; slopes and dimensions shall comply with slopes and dimensions shown on the drawing, or to the maximum extent feasible permitted by the ENGINEER. Final configuration of the assembly may be different than shown. Where physical constraints (e.g. utility covers, poles, vaults, etc.) prevent compliance, a single diagonal curb cut assembly may serve both pedestrian street crossings.
- B. Installation of flares or curb returns is ENGINEER's choice.
- C. Definitions and supplemental requirements are specified in APWA Section 32 16 14.

2. PRODUCTS

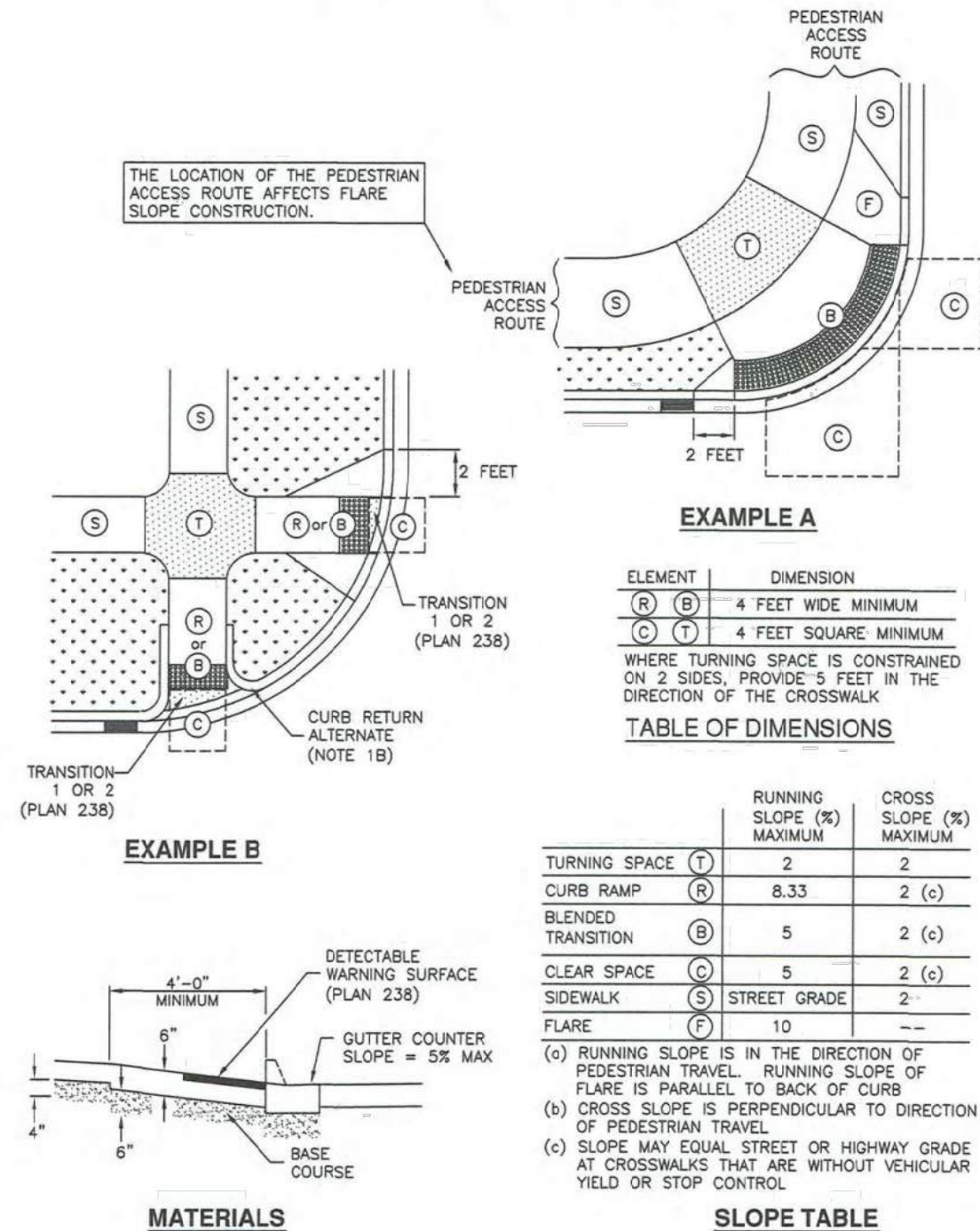
- A. Base Course: Untreated base course, APWA Section 32 11 23. Do not use gravel as a base course without ENGINEER's permission.
- B. Expansion Joint Filler: 1/2-inch thick type F1 full depth, APWA Section 32 13 73.
- C. Detectable Warning Surface: Paver, ribbed composite panel, or tile. Provide a color that contrasts with adjacent walking surface, either light-on-dark or dark-on-light. ENGINEER to select type and color unless indicated elsewhere.
- D. Concrete: Class 4000, APWA Section 03 30 04.
- E. Concrete Curing Agent: Clear membrane forming compound with fugitive dye (Type ID Class A), APWA Section 03 39 00.

3. EXECUTION

- A. Base Course Placement: APWA Section 32 05 10. Maximum lift thickness before compaction is 8-inches when using riding equipment or 6-inches when using hand held equipment. Compaction is 95 percent or greater relative to a modified proctor density, APWA Section 31 23 26.
- B. Curb Modifications:
 - 1) The sloped surface created to accommodate a flare area shall be perpendicular to the back of curb.
 - 2) No grade break shall exist between the flow-line and the foot of the curb ramp or blended transition. Length of the curb modification abutting the curb ramp or transition is 4 feet minimum for each crosswalk served.
- C. Curb Ramp: Length not required to exceed 15 feet. Grade breaks are perpendicular to the direction of ramp run and are not permitted on ramp or turning space surface. Sides are parallel to each other and perpendicular to the ends. At the bottom grade break it may be necessary to install a transition zone, (APWA Plan 238).
- D. Concrete Placement: APWA Section 03 30 10.
 - 1) Maximum length to width ratio for rectangular panel joints is 1.5 to 1. Joint spacing measured in feet not to exceed twice slab thickness measured in inches or a maximum of 15 feet.
 - 2) Install expansion joints vertical, full depth, with top of filler set flush with concrete surface. Install contraction joints vertical, 1/8-inch wide, and 1/4 of the depth of the concrete flatwork.
 - 3) Provide 1/2-inch radius edges. Apply a broom finish. Apply a curing agent.
- E. Clear Space: No trip hazards in the clear space.

235.1

TURNING SPACE AT SIDEWALK LEVEL



Corner curb cut assembly

Plan
235.1
September 2011

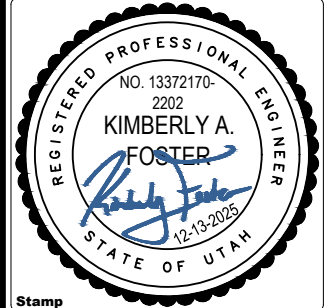
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ROADWAY DETAILS

VINEYARD CITY

**400 S RECONSTRUCTION
(HOLDAWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

SHEET NO. 12 OF 18

SHEET TITLE **RDT-03**

Detectable warning surface

1. GENERAL

- A. Detectable warnings consist of a surface of truncated domes aligned in a square or radial grid pattern with dome size, dome spacing, contrast and panel size as indicated.
- C. Definitions and supplemental requirements are specified in APWA Section 32 16 14.

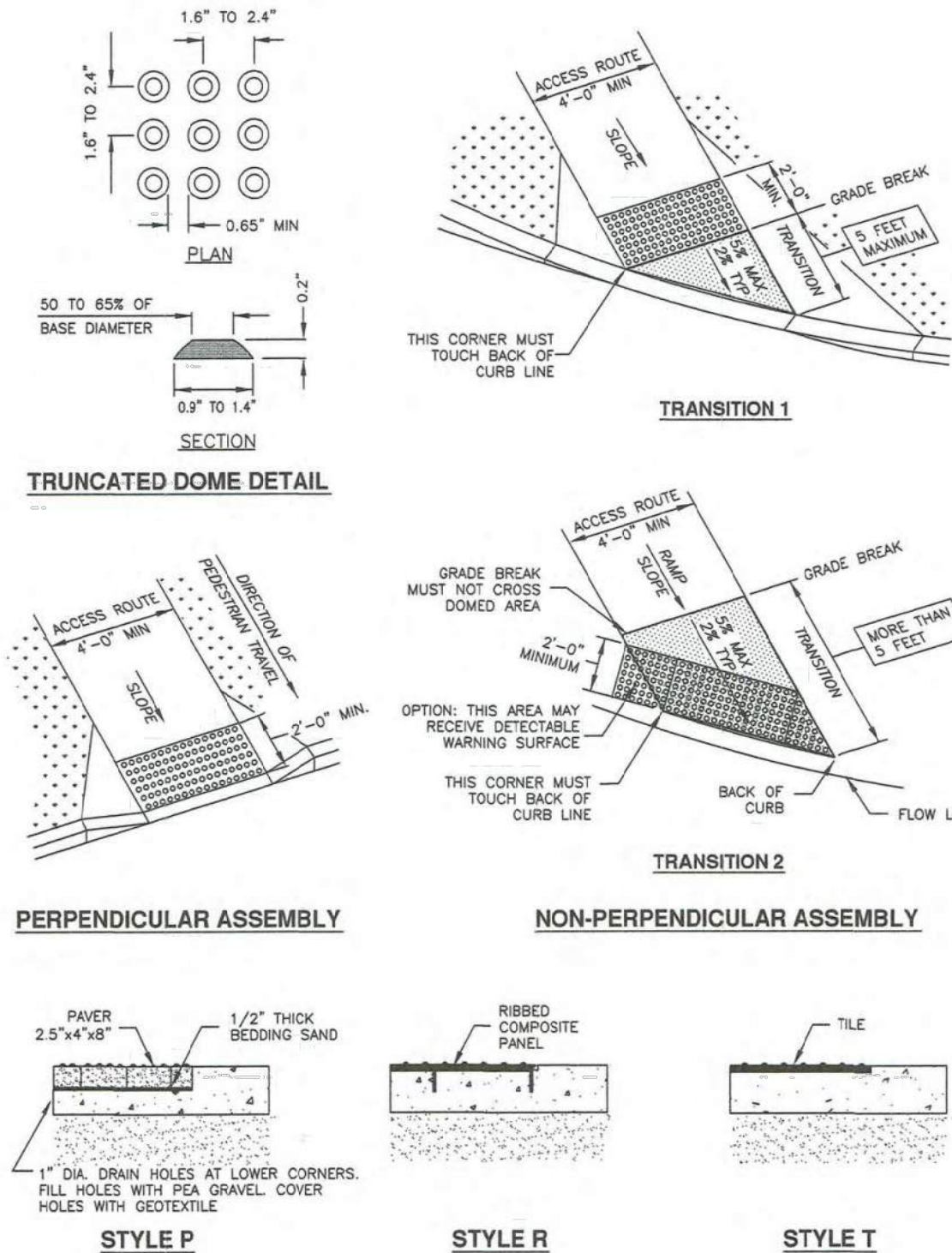
2. PRODUCTS

- A. Pavers:
 - 1) Concrete, APWA Section 32 14 13.
 - 2) Brick and Mortar, APWA Section 32 14 16.
- B. Tile: Unless indicated elsewhere, selection is by CONTRACTOR as allowed by ENGINEER.
- C. Ribbed Composite Panel: Unless indicated elsewhere, selection is by CONTRACTOR as allowed by ENGINEER.
- D. Bedding Sand, Joint Sand, Geotextile: APWA Section 32 14 13.

3. EXECUTION

- A. Layout:
 - 1) Joints Between Units: 3/16 inch maximum or manufacturer's recommendation.
 - 2) Flares: Do not install detectable warning units on flared surfaces.
 - 3) Alignment: Where a ramp, turning space, or blended transition provides access to the street continuously around a corner, align the vertical rows of truncated domes to be perpendicular or radial to the grade break between the ramp and the street for a 4 feet minimum width for each crosswalk served.
 - 4) Transition 1 or 2: Selection is by ENGINEER unless indicated elsewhere.
 - 5) At Rail Crossings: The edge of the detectable warning surface nearest the rail crossing is 6 feet minimum and 15 feet maximum from the centerline of the nearest rail.
- B. Paver Installation: APWA Section 32 14 13. If paver must be cut, minimum paver cut length is 3/4 paver, or 1/2 paver length providing the adjacent paver is also reduced no more than 1/2 its original length. Do not cut pavers longitudinally. Remove domes that were cut.
- C. Tile Installation: Install according to manufacturer's recommendations. Remove domes that were cut.
- D. Ribbed Composite Panel Installation: Install according to manufacturer's recommendation. Remove domes that were cut. Seal cuts to prevent water intrusion.

238



Detectable warning surface

Plan
238
July 2011

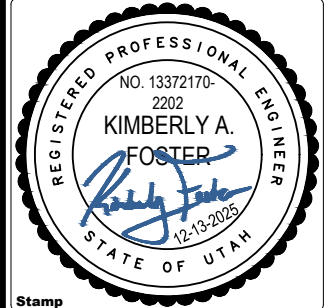
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ROADWAY DETAILS

VINEYARD CITY

**400 S RECONSTRUCTION
(HOLDWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer KF, PE	Checker KC, PE
SHEET NO.	13 OF 18
SHEET TITLE	RDT-04

Curb and gutter replacement without pavement tie-in

1. GENERAL

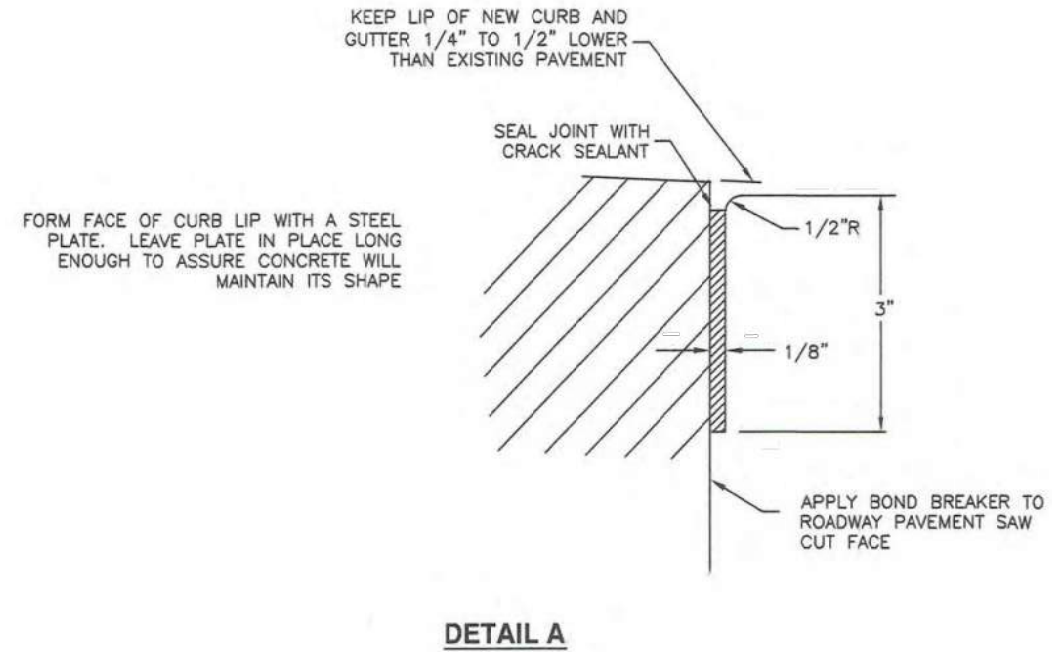
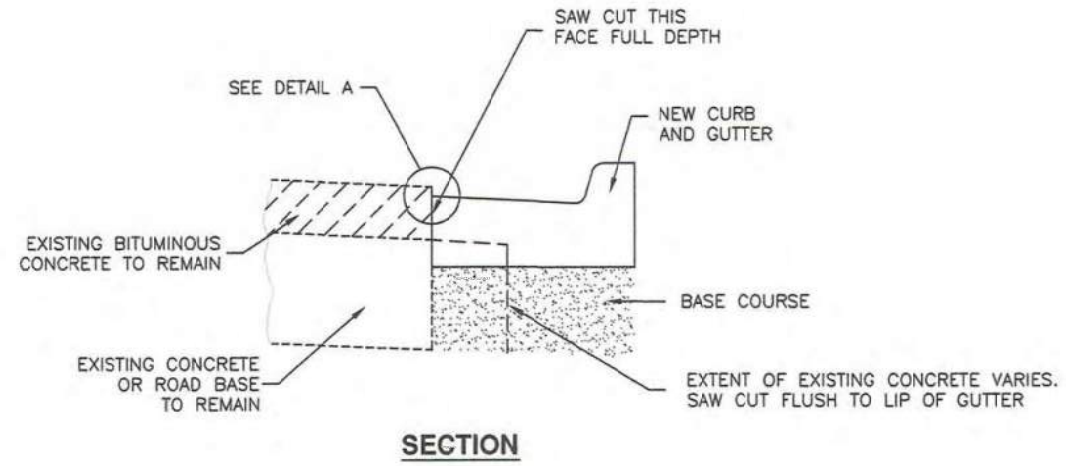
A. Place curb and gutter or other concrete flatwork against existing pavement

2. PRODUCTS

- A. Base Course: Untreated base course, APWA Section 32 11 23. Do not use gravel as a base course without ENGINEER's permission.
- B. Bond Breaker: Paraffin wax, lithium grease, or other semi-solid, inert lubricant.
- C. Crack Sealant: CAS1, APWA Section 32 13 73.

3. EXECUTION

- A. Base Course Placement: Match existing thickness or 8-inches minimum. Maximum lift thickness before compaction is 8-inches when using riding equipment or 6-inches when using hand held equipment. Compaction is 95 percent or greater relative to a modified proctor density, APWA Section 31 23 26.
- B. Apply bond breaker to vertical surface of roadway pavement cut.
- C. Steel Plate.
 - 1) A steel plate provides a clean straight edge so bituminous concrete can be milled out in the future without damaging the curb and gutter.
 - 2) If straight edge between Portland-cement concrete and bituminous concrete cannot be provided, remove and replace curb and gutter per Plan 251.
 - 3) Remove the plate after concrete has set.



Curb and gutter replacement without pavement tie-in

Plan
252
August 2001

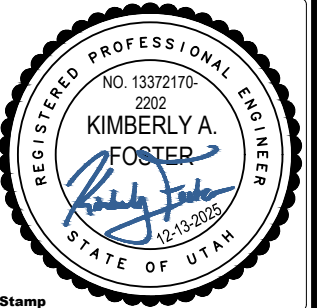
252

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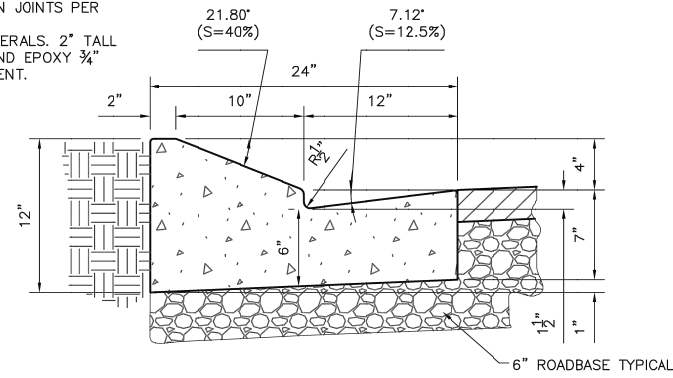


ROADWAY DETAILS
VINEYARD CITY

400 S RECONSTRUCTION (HOLDAWAY RD TO LAKEVIEW DR)
125 SOUTH MAIN STREET

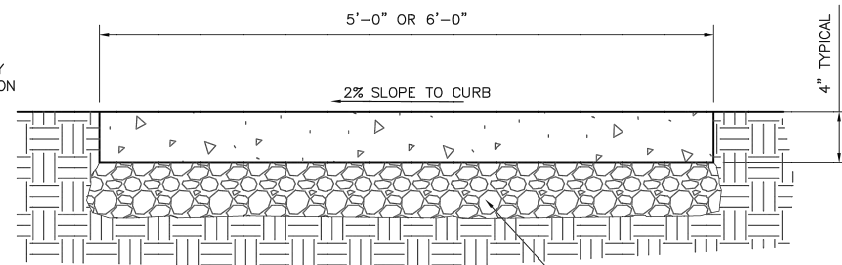
Designer KF, PE	Checker KC, PE
SHEET NO.	14 OF 18
SHEET TITLE	RDT-05

NOTE:
1. CONTRACTION AND EXPANSION JOINTS PER APWA STANDARDS.
2. S, W, OR L MARKER FOR LATERALS. 2" TALL WET SET LETTER OR DRILL AND EPOXY 3/4" BRASS WITH 1" MIN. EMBEDMENT.



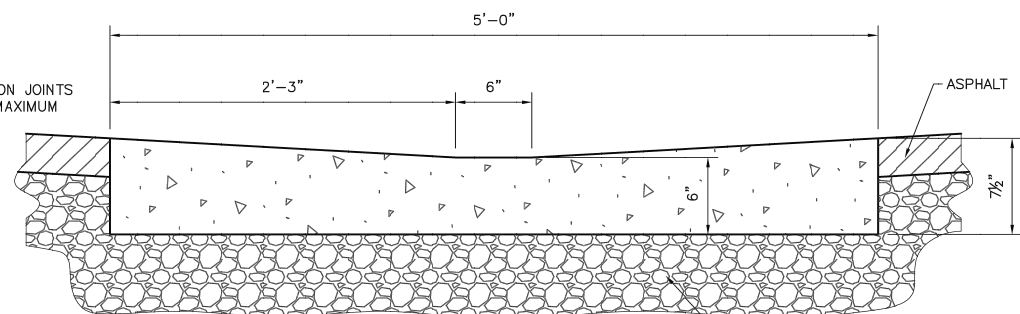
MOUNTABLE CURB

NOTE:
CONTRACTION JOINTS EVERY 10' MAXIMUM AND EXPANSION JOINTS EVERY 50' MAXIMUM



SIDEWALK SECTION

NOTE:
CONTRACTION JOINTS EVERY 5' MAXIMUM



CROSS GUTTER SECTION

NOTE:
1. CROSS GUTTERS ARE NOT PERMITTED WITHOUT TOWN ENGINEER APPROVAL.



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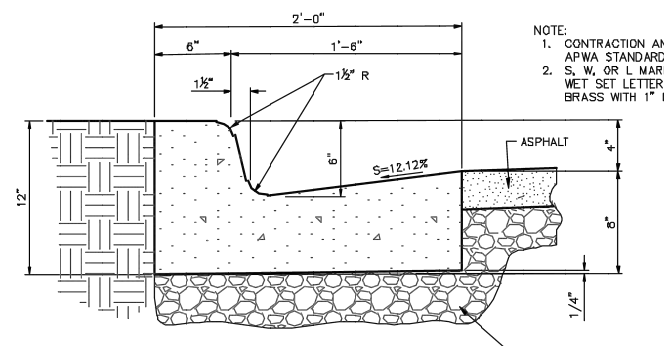


CURB, GUTTER, AND SIDEWALK DETAILS

TOWN OF VINEYARD ORIGINAL

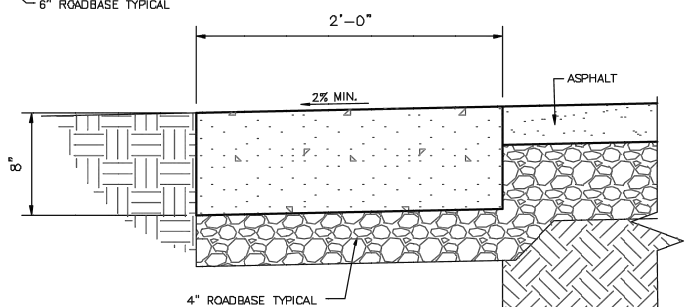
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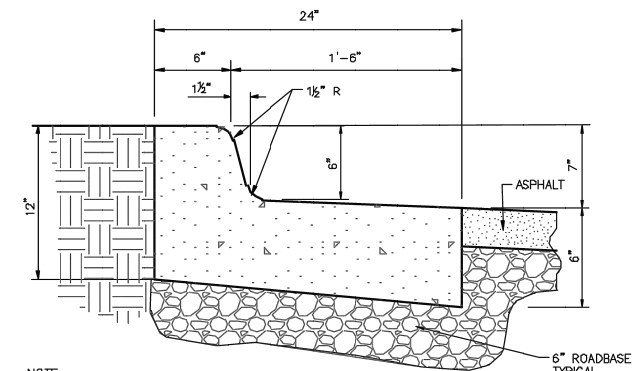
TYPICAL CURB & GUTTER

NOTE:
CONTRACTION JOINTS EVERY 10' MAXIMUM AND EXPANSION JOINTS EVERY 50' MAXIMUM.



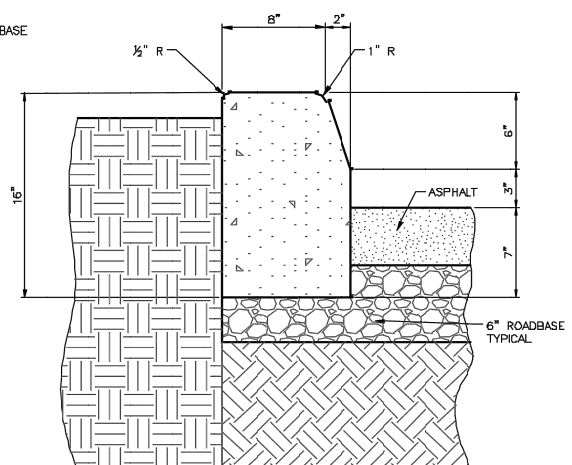
FLAT CURB BORDER

NOTE:
CONTRACTION JOINTS EVERY 10' MAXIMUM AND EXPANSION JOINTS EVERY 50' MAXIMUM



NON-CARRY TYPE CURB & GUTTER

NOTE:
CONTRACTION JOINTS EVERY 10' MAXIMUM AND EXPANSION JOINTS EVERY 50' MAXIMUM



BARRIER CURB



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CURB DETAILS

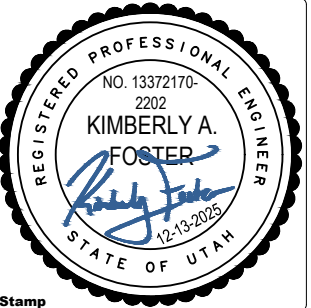
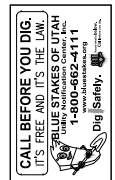
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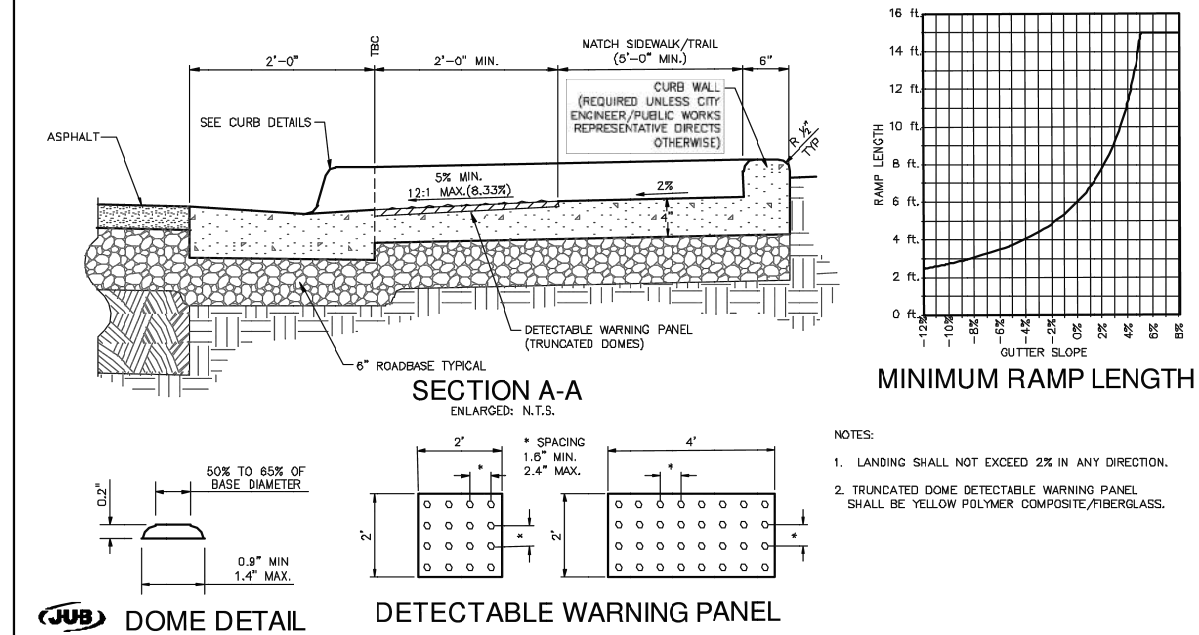
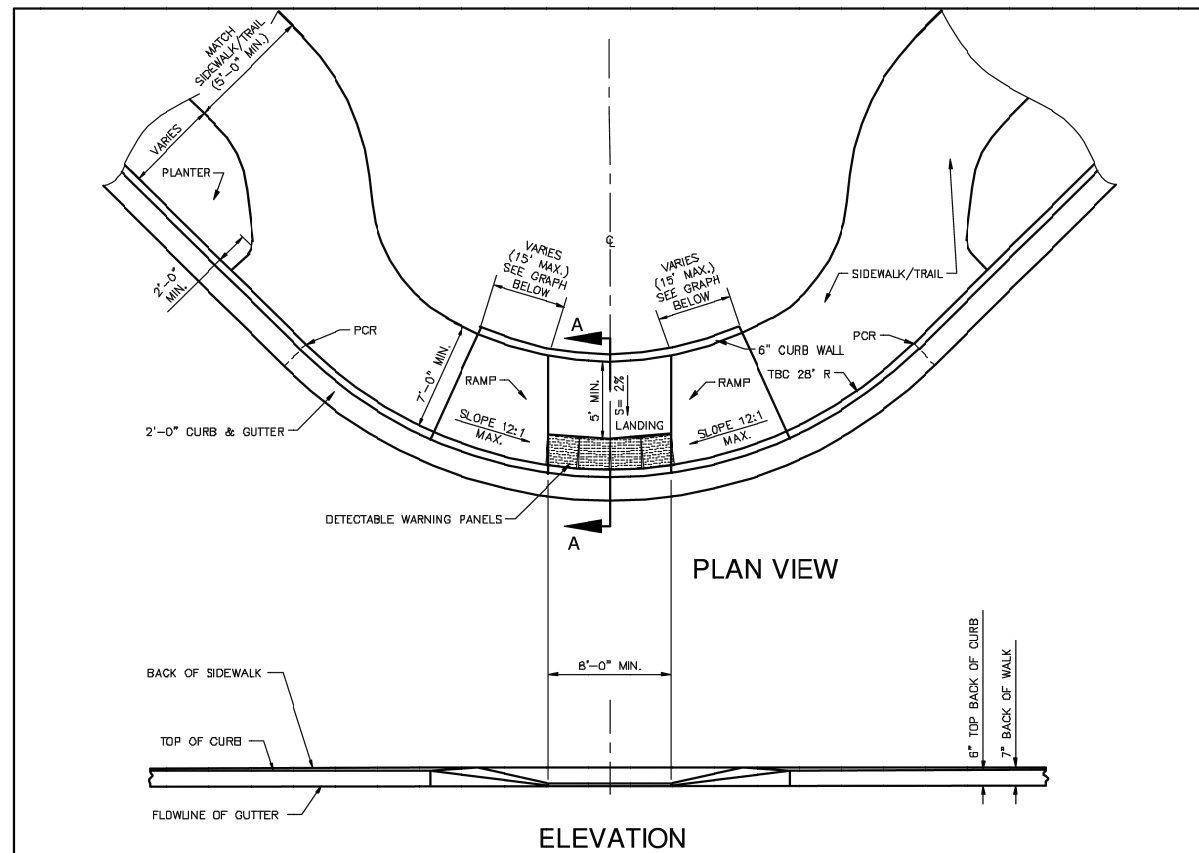


ROADWAY DETAILS
VINEYARD CITY
400 S RECONSTRUCTION (HOLDADWAY RD TO LAKEVIEW DR)
125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

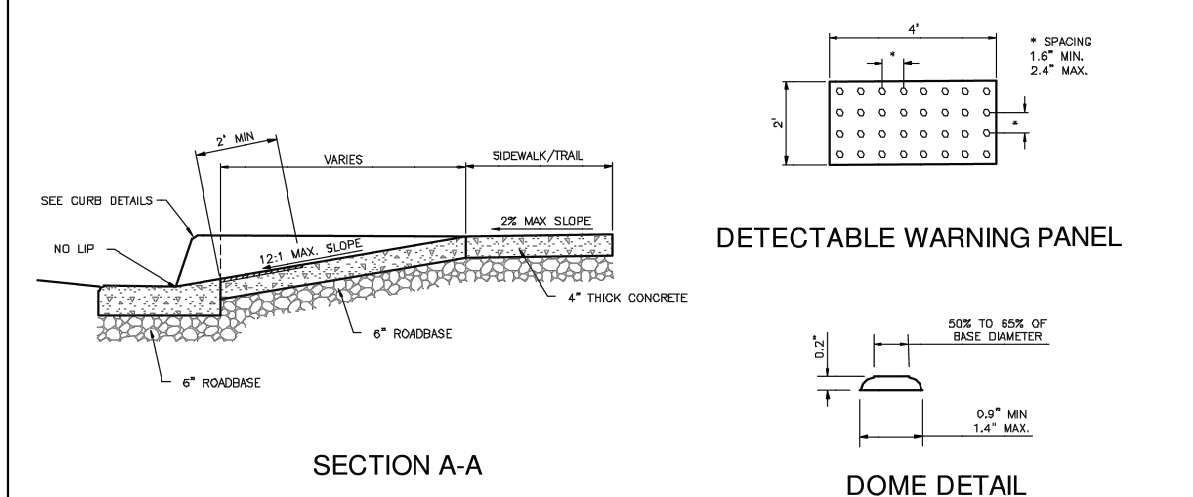
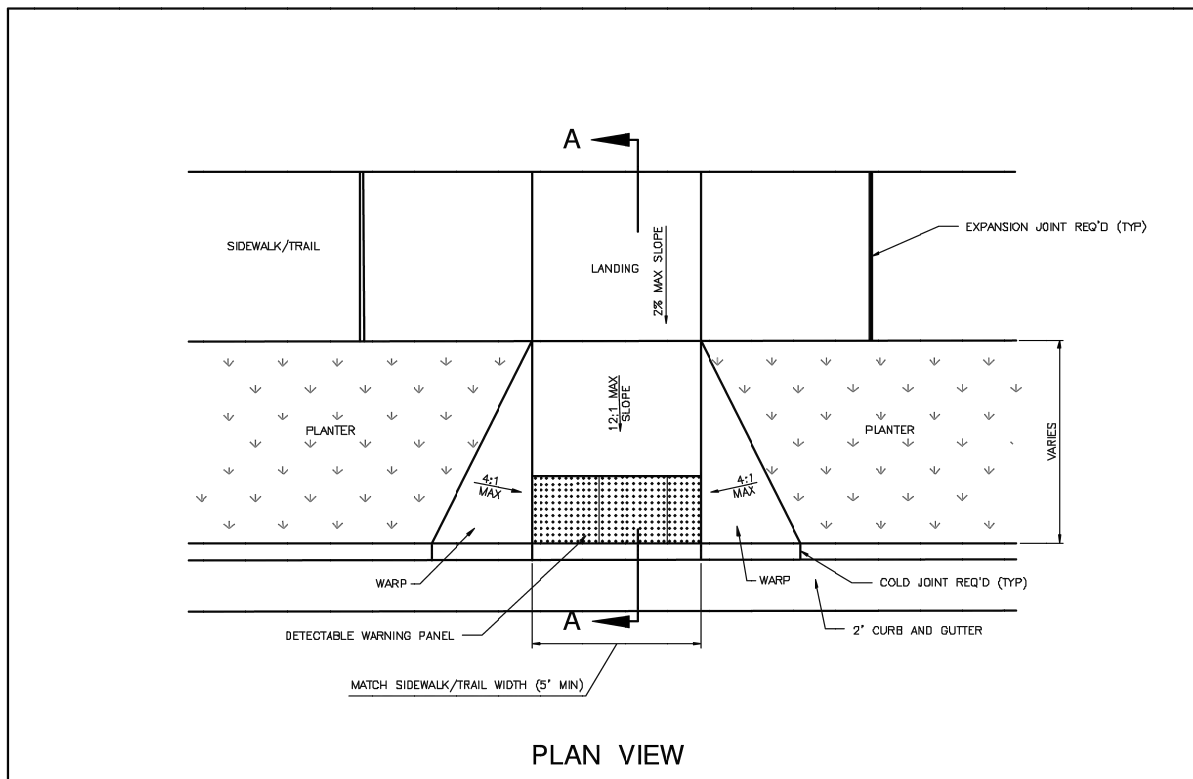
SHEET NO. 15 OF 18

SHEET TITLE **RDT-06**



- NOTES:
1. LANDING SHALL NOT EXCEED 2% IN ANY DIRECTION.
 2. TRUNCATED DOME DETECTABLE WARNING PANEL SHALL BE YELLOW POLYMER COMPOSITE/FIBERGLASS.

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REVISION		TOWN OF VINEYARD ORIGINAL		SEPTEMBER 1, 2016	
NO.	DESCRIPTION	BY	APPR. DATE	NO.	DESCRIPTION
1	REVISION	JM	09/28/21	1	REVISION
2				2	
3				3	



- NOTES:
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 2. TRUNCATED DOME DETECTABLE WARNING PANEL SHALL BE YELLOW POLYMER COMPOSITE/FIBERGLASS.

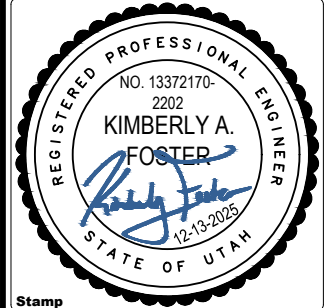
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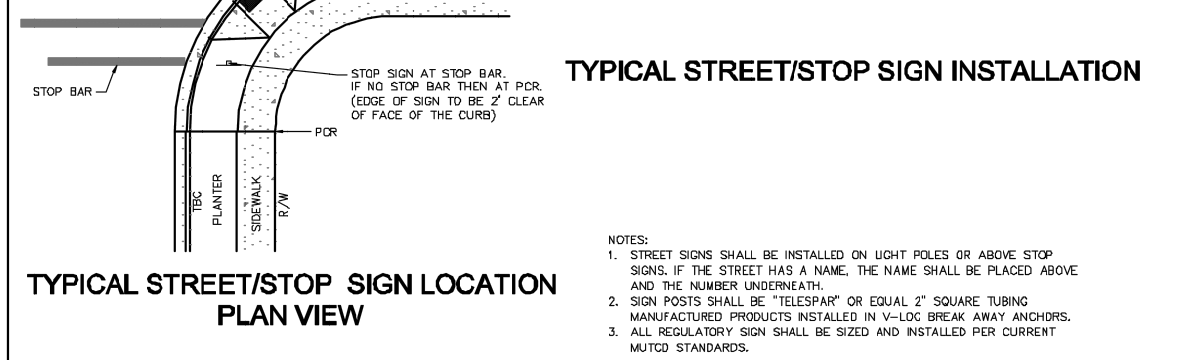
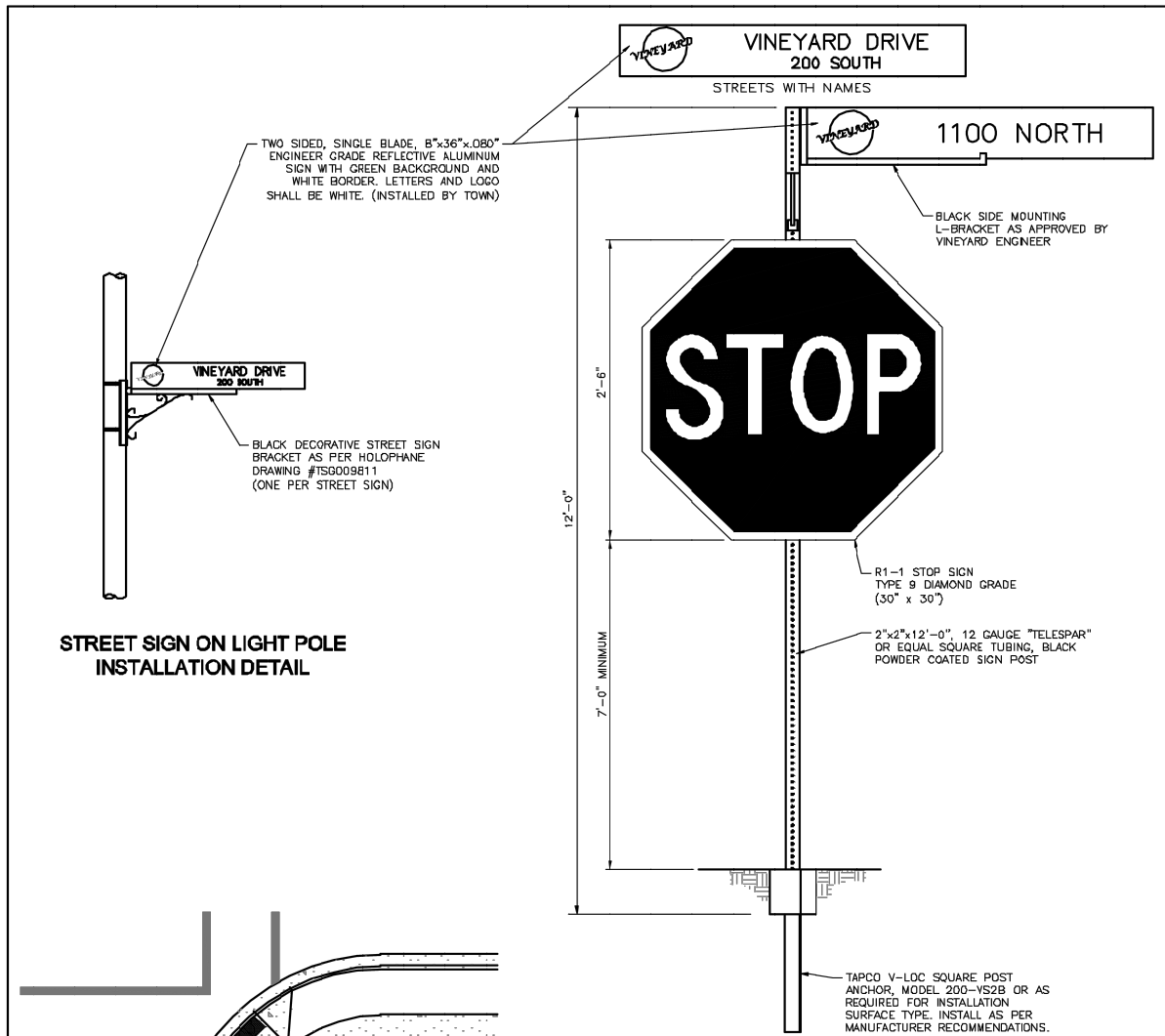
ROADWAY DETAILS

VINEYARD CITY

**400 S RECONSTRUCTION
(HOLDADWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

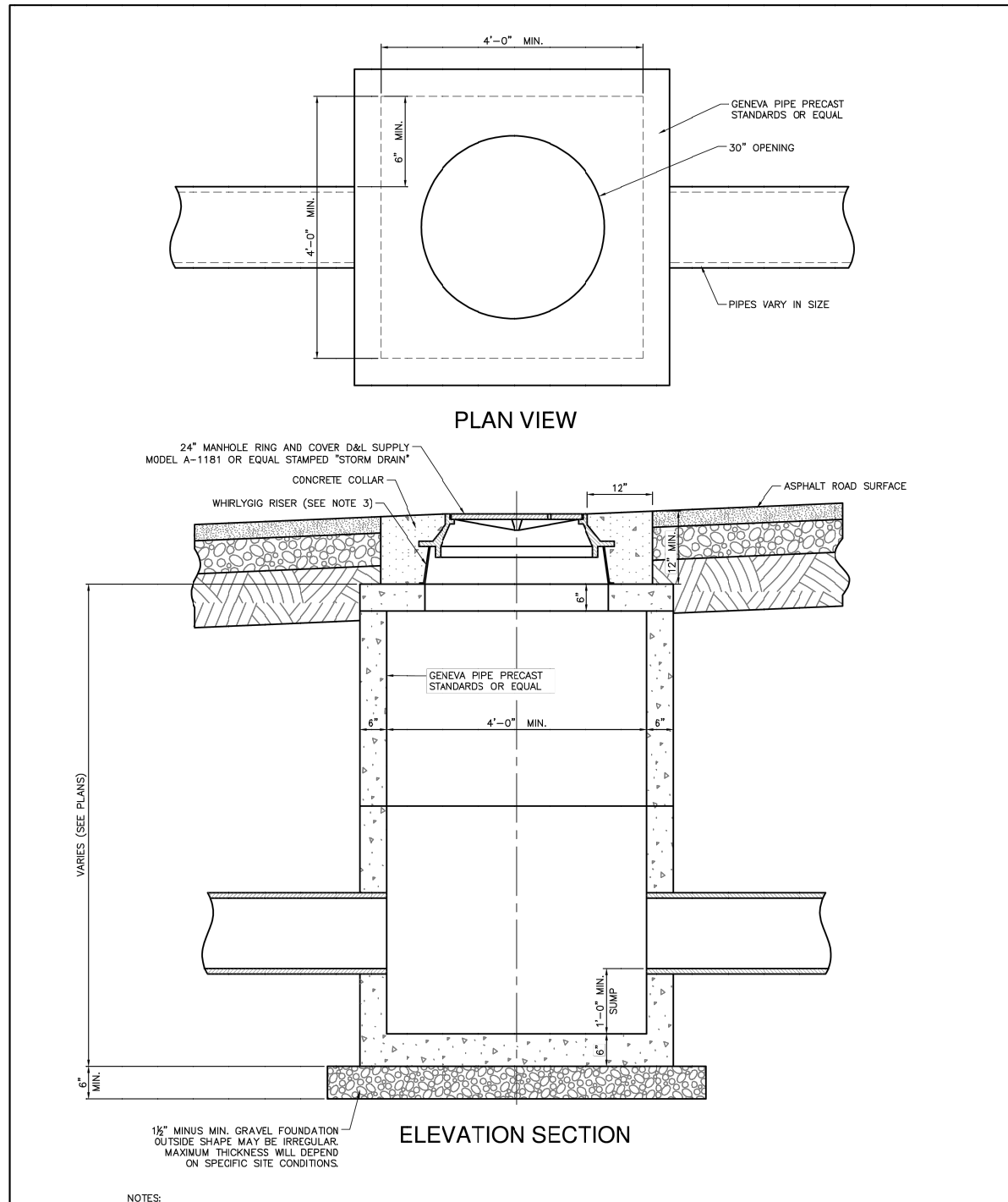
Designer	KF, PE	Checker	KC, PE
SHEET NO.	16 OF 18		
SHEET TITLE	RDT-07		



NOTES:
 1. STREET SIGNS SHALL BE INSTALLED ON LIGHT POLES OR ABOVE STOP SIGNS. IF THE STREET HAS A NAME, THE NAME SHALL BE PLACED ABOVE AND THE NUMBER UNDERNEATH.
 2. SIGN POSTS SHALL BE "TELESPAR" OR EQUAL 2" SQUARE TUBING MANUFACTURED PRODUCTS INSTALLED IN V-LOG BREAK AWAY ANCHORS.
 3. ALL REGULATORY SIGN SHALL BE SIZED AND INSTALLED PER CURRENT MUTCD STANDARDS.

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REVISION		DESIGN BY: JMF CHECKED BY: DMO ADOPTED DATE: SEPTEMBER 1, 2016	
NO.	DESCRIPTION	BY	DATE
1	ISSUED FOR PERMITS	JMF	12/13/2025
2			
3			

TOWN OF VINEYARD
ORIGINAL
 SEPTEMBER 1, 2016



NOTES:
 1. MANHOLE COVER TO BE A SOLID LID CONTAINING ONE PICK HOLE. SEE SPECIFICATION DIVISION 5 FOR COVER LABEL. D&L SUPPLY P/N A-1181 (OR EQUAL).
 2. FLAT LIDS SHALL MEET H20 LIVE LOADING. NO FLAT RING AND COVERS WILL BE ALLOWED UNLESS APPROVED BY TOWN ENGINEER.
 3. WHIRLYGIG RISER OR EQUAL SHALL BE USED INSTEAD OF GRADE RINGS TO ADJUST MANHOLE RING AND COVER TO GRADE.
 4. 18" MAXIMUM WHIRLYGIG RISER HEIGHT.

STATEMENT OF USE		STANDARD DRAWING NUMBER: 9A	
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REVISION		DESIGN BY: JMF CHECKED BY: DMO ADOPTED DATE: SEPTEMBER 1, 2016	
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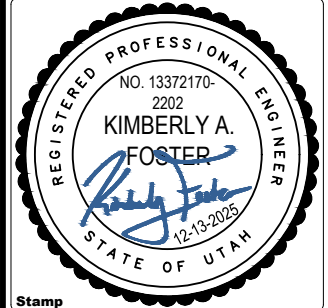
TOWN OF VINEYARD
ORIGINAL
 SEPTEMBER 1, 2016

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ROADWAY DETAILS

VINEYARD CITY

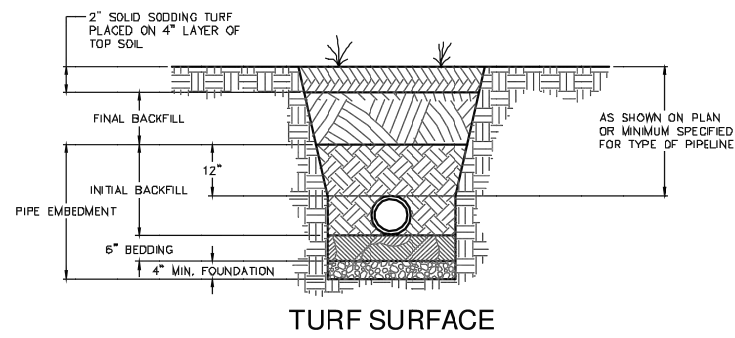
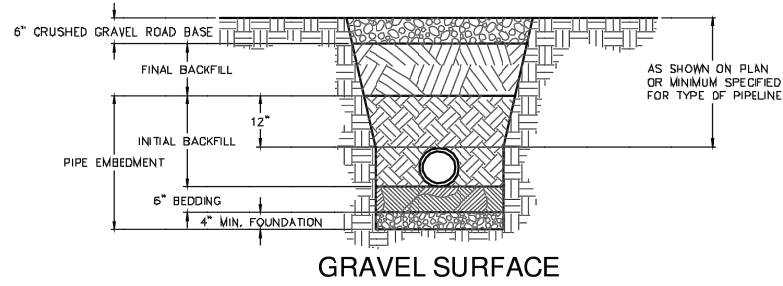
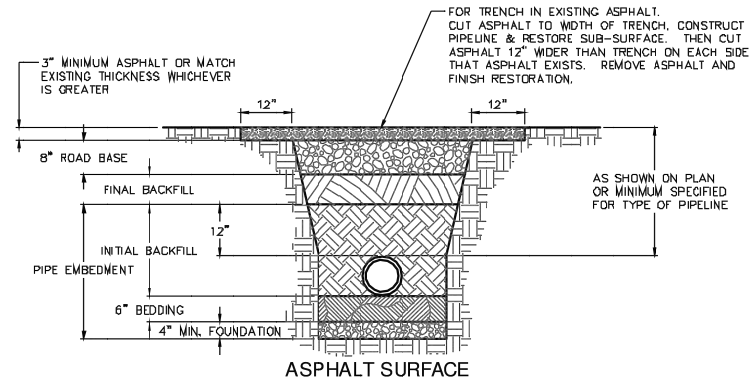
**400 S RECONSTRUCTION
 (HOLDAWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer KF, PE Checker KC, PE

SHEET NO. 17 OF 18

SHEET TITLE **RDT-08**



- NOTES:
1. SLOPE TRENCH TO MEET OSHA REQUIREMENTS (LATEST EDITION) OR USE TRENCH BOX.
 2. FOUNDATION AND BEDDING MATERIAL AS REQUIRED.
 3. INSTALL PIPELINES ON STABLE FOUNDATION WITH UNIFORM BEARING FOR FULL LENGTH OF BARREL, EXCAVATE IN BEDDING FOR ALL PIPE JOINTS.
 4. NO PATCHES WITHIN EXISTING PATCHES.

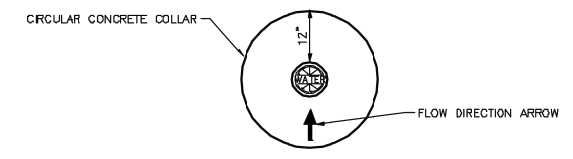
TYPICAL TRENCH SECTION

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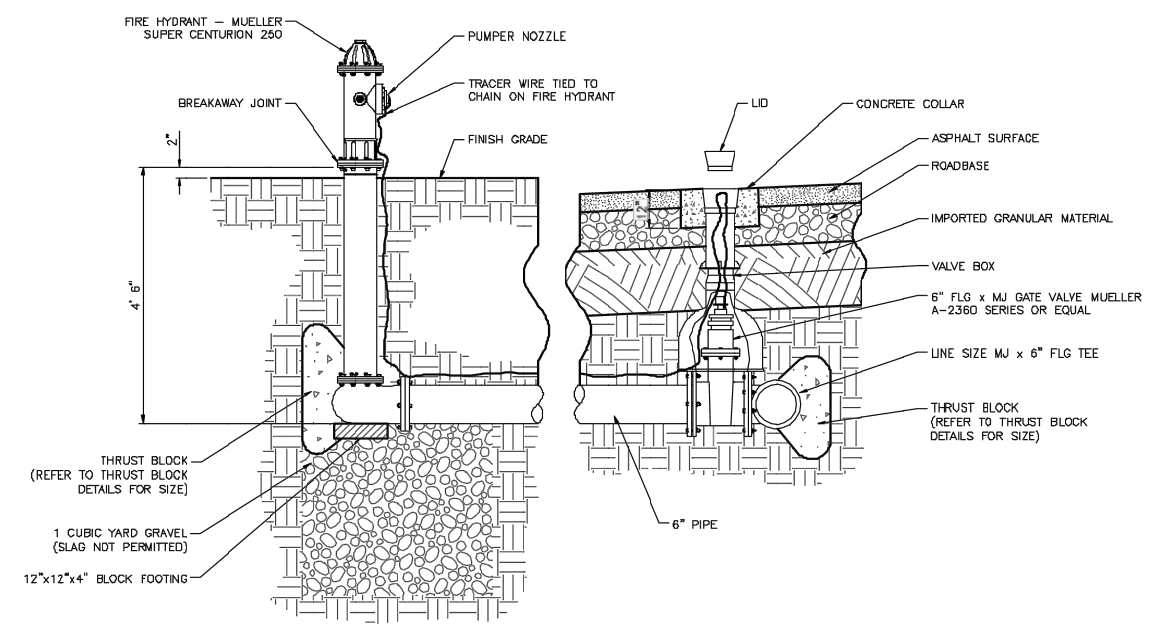
NO.	REVISION	BY	DATE
1	DESIGNED BY	JAM	09/28/24
2	CHECKED BY	DDO	
3	ADOPTED DATE:		SEPTEMBER 1, 2016

DESIGNER: JAM
CHECKER: DDO
DATE: SEPTEMBER 1, 2016

TOWN OF VINEYARD ORIGINAL



CULINARY WATER VALVE
CONCRETE COLLAR



FIRE HYDRANT & WATER VALVE

- NOTES:
1. BOLTS: APPLY ONE (1) COAT OF FW2 GREASE (FOOD GRADE) TO ALL EXPOSED SURFACES OF BOLTS AND TO ALL BOLT THREADS AFTER INSTALLATION OF PIPING, FITTINGS, VALVES, AND COUPLINGS.
 2. VALVES, FITTINGS, CONNECTIONS, ETC.: ENCASE ALL BURIED DUCTILE IRON VALVES, FITTINGS, CONNECTIONS AND SPECIALTIES IN MINIMUM 8 MIL. BLACK POLYETHYLENE SHEETS IN ACCORDANCE WITH AWWA C-105. SECURE POLYETHYLENE SHEETS TO THE PIPE AS SPECIFIED ABOVE.
 3. DUCTILE IRON PIPE: ENCASE BURIED DUCTILE IRON PIPE IN MINIMUM 8 MIL. POLYETHYLENE SHEETS IN ACCORDANCE WITH AWWA C105 IN ALL AREAS AND SOIL TYPES AS SHOWN ON THE PROJECT PLANS OR AS SPECIFIED BY THE ENGINEER.
 4. NO RISER KITS SHALL BE INSTALLED ABOVE FINISHED GRADE.

FIRE HYDRANT AND WATER VALVE DETAIL

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DESIGNER: JAM
CHECKER: DDO
DATE: SEPTEMBER 1, 2016

TOWN OF VINEYARD ORIGINAL

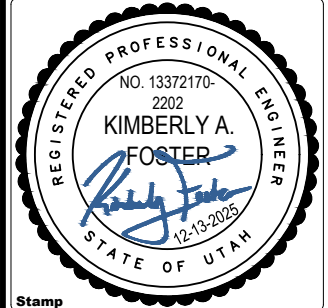
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ROADWAY DETAILS

VINEYARD CITY

**400 S RECONSTRUCTION
(HOLDAWAY RD TO LAKEVIEW DR)**

125 SOUTH MAIN STREET

Designer	KF, PE	Checker	KC, PE
SHEET NO.	18 OF 18		
SHEET TITLE	RDT-09		

RESOLUTION NO. 2026-31

A RESOLUTION OF THE VINEYARD CITY COUNCIL ADOPTING A POLICY FOR THE DISPOSAL OF INSIGNIFICANT MUNICIPAL REAL PROPERTY AND THE RESOLUTION OF PRE-2020 RIGHT-OF-WAY ENCROACHMENTS PURSUANT TO UTAH CODE ANN. § 10-8-2.

WHEREAS, Vineyard City ("the City") owns real property, including various public rights-of-way, public utility easements, and surplus remnants acquired over time during the city's transition from a rural to an urbanized community; and

WHEREAS, from time to time, the City discovers legacy or historical encroachments where fences, secondary structures, or improved landscaping belonging to adjacent private landowners extend into City-owned right-of-way or property; and

WHEREAS, many of these technical encroachments occurred prior to January 1, 2020, during earlier phases of local subdivision development; and

WHEREAS, Utah Code Ann. § 10-8-2(4) mandates that a municipality may not dispose of a "significant parcel of real property" without first providing public notice and holding a public hearing, but allows municipalities the legislative authority to define what constitutes an *insignificant* parcel exempt from such multi-week procedural requirements; and

WHEREAS, the Vineyard City Council finds that reducing administrative burdens for minor property line adjustments involving land with an underlying value of less than \$20,000 and an area under 300 square feet serves the public interest when such adjustments resolve existing encroachments and provide a mutual benefit to both the City and adjacent property owners; and

WHEREAS, the City Council desires to establish a clear policy granting itself legislative discretion to evaluate and resolve these minor encroachments on a case-by-case basis while maintaining strict compliance with the Utah Constitution's limits on the lending of public credit by utilizing cost-benefit offsets and intangible value considerations.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF VINEYARD, UTAH AS FOLLOWS:

SECTION 1: Adoption.

The Vineyard City Council hereby adopts the *"Insignificant Land Disposal and Encroachment Resolution Policy,"* attached hereto as Exhibit "A" and incorporated by this reference.

SECTION 2: Definition of Insignificant Property.

Pursuant to Utah Code Ann. § 10-8-2(4), the City Council formally defines an "Insignificant Parcel" as any municipal real property interest, right-of-way remnant, or portion thereof that has a fair market value of less than \$20,000, is independently unbuildable, under 300 square feet in size, does not serve any transportation benefit, and possesses no distinct operational utility to Vineyard City.

SECTION 3: Legislative Findings Required.

In exercising its discretion to adjust disposal costs or accept intangible value as consideration for an insignificant parcel, the City Council shall make explicit findings on the record during a public meeting identifying the public benefits, shifted liabilities, or administrative savings achieved by the transfer.

SECTION 4: Severability.

If any section, clause, or provision of this Resolution or the associated policy is declared invalid by a court of competent jurisdiction, the remaining portions shall remain in full force and effect.

SECTION 5: Effective Date.

This Resolution shall take effect immediately upon its passage.

PASSED AND ADOPTED by the City Council of Vineyard, Utah, this ____ day of _____, 2026.

APPROVED:

_____, Mayor

ATTEST:

_____, City Recorder

EXHIBIT "A"

INSIGNIFICANT LAND DISPOSAL AND ENCROACHMENT RESOLUTION POLICY

I. Application and Eligibility

This policy applies exclusively to applications by adjacent private primary-residential property owners seeking to quiet title or purchase City-owned property fragments where an encroachment exists. To qualify for Council cost-discretion under this policy, the applicant must demonstrate to the satisfaction of City Staff and the City Council that:

1. The real property asset value is less than \$20,000 as determined by internal data or a licensed appraisal.
2. The real property asset value is less than 300 square feet in size as determined by internal data or a licensed appraisal.
3. The encroachment physically occurred prior to January 1, 2020, or represent contiguous parcels that impact a single adjacent property line at the time when a request for a contiguous pre-2020 property is being addressed.
4. The encroachment resulted from a good-faith mistake or was inherited by the current property owner.

Meeting eligibility does not create a right to the purchase or conveyance of property and remains subject to approval by the governing body.

II. Cost Considerations

In determining the final financial consideration required from the applicant, the City Council may, in its legislative discretion, discount the baseline cash purchase price by formally crediting the following factors:

- **Shifted Liability:** Financial credit given for the permanent elimination of City maintenance liabilities, including weed control, stormwater drainage risks, or public safety monitoring.
- **Intangible Public Benefit:** Financial credit given for the resolution of legacy boundary disputes, avoidance of potential quiet-title litigation costs, or the formalization of long-standing residential boundaries that bring the neighborhoods into consistent alignment.
- **Administrative Floor:** The applicant shall bear all direct third-party transactional costs incurred by the City, including survey validation, legal review, and recording fees. The applicant may bear any staff costs at the discretion of the council.

III. Reservation of Rights and Easements

No disposal of any right-of-way segment under this policy shall be finalized without a comprehensive utility review. If active or future culinary water, secondary water, sewer, stormwater, or third-party franchise utility corridors exist within the footprint, Vineyard City **shall reserve a perpetual Public Utility Easement (PUE)** across the conveyed footprint as a condition of deed transfer.

IV. Approval

All findings shall receive a staff recommendation and be reviewed in an Open Public Meeting for approval or denial by the city council. Applications may receive a partial reduction in Shifted Liability or Intangible Public Benefit approval as determined by the governing body. As each situation is unique, approval of one application shall not establish a precedence or automated approval or consideration of additional applications. Upon approval of the application, and prior to the disposition of public property, Vineyard City shall comply with Utah Code Ann. § 11-1-203(3) for 14 consecutive days.